

City of Albuquerque

North I-25 Sector Plan Development Code Update

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1.1 INTENT

The purpose of this Overlay Code is to enable, encourage and qualify the implementation of the following policies:

1.1.1 Transect-based Code

Many of the most-loved traditional towns of New Mexico were deliberately and thoughtfully planned. Countless other cities, towns, and villages evolved as compact, walkable, mixed-use places, because of their geography and because of the limits of the economic and circumstances of their time. However, in our time, over the past sixty years, places have evolved in a completely different form. They have spread loosely along highways and haphazardly across once-open country, enabled by the widespread ownership of automobiles, cheap petroleum, and generalized wealth.

The corresponding codes incorporate zoning practices that separate our homes from offices, shops, churches, and schools. They include design standards that favor the automobile over the pedestrian. They respond to the homogenizing effects of globalization.

These practices, since World War II, have produced strip shopping, big box stores with enormous parking lots, and sadly gutted downtowns. They have produced tracts of banal housing that consume farmland and forests. They have produced the invention and proliferation of drive-by eateries and billboards. They have made walking or cycling beyond one's own cul-de-sac dangerous or even impossible. They have made children, the elderly, and the poor dependent on those who can drive. There has been simultaneous destruction of both towns and open space -- the 20th Century phenomenon known as sprawl.

The form of our built environment needs a 21st Century correction. But in most places, it is actually illegal to build a traditional town or neighborhood like those where our grandparents lived. The existing codes prevent it. In most places, people do not have a choice between sprawl and traditional urbanism. Economics and politics favor sprawl and conventional suburban development (CSD). It is not a level playing field that preserves the form of urbanism and open lands.

The I-25 Sector Code is a Transect-based code. That means it organizes the natural, rural, suburban, and urban landscape into categories of density, complexity, and intensity in the same way the countryside relates to the traditional towns and villages we admire. The operating principle of the Transect is that certain forms belong in certain environments; for example, an apartment building belongs in a more urban setting, and a ranch house belongs in a more rural setting.

Some kinds of thoroughfares are urban, and some are rural. This does not limit choices; it expands them.

Instead of one-size-fits-all development, it enables different patterns, without becoming a free-for-all. The code, once adopted as law, will evolve and mature without losing its sense of order.

1.1.2 The I-25 Sector

- a. That Neighborhoods, Corridors, Centers, and Districts should be compact, pedestrian-oriented and mixed-use.
- b. That Neighborhoods, Corridors and Centers should be the preferred pattern of development and that districts specializing in special or single-use should be a necessary exception.
- c. That ordinary activities of daily living should occur within walking distance of most dwellings and employment areas, allowing independence to those who do not drive.
- d. That interconnected networks of Thoroughfares should be designed to disperse and reduce the length of automobile trips.
- e. That within Neighborhoods, a range of housing Types and price levels should be provided to accommodate diverse ages and incomes.
- f. That appropriate building Densities and land uses should be provided within walking distance of transit stops.
- g. That new schools should be sized and located to enable children to walk or bicycle to them.
- h. That a range of open space including parks, squares, and playgrounds should be distributed within Neighborhoods and urban center zones.

1.1.3 The I-25 Sector's Blocks and Buildings

- a. That buildings and landscaping should contribute to the physical definition of Thoroughfares as Civic places.
- b. That development should adequately accommodate automobiles while respecting the pedestrian and the spatial form of public space.
- c. That the design of streets and buildings should reinforce safe environments, but not at the expense of accessibility.
- d. That architecture and landscape design should grow from local climate, topography, history, and building practice.
- e. That buildings should provide their inhabitants with a clear sense of geography and climate through energy efficient methods.

- f. That Civic Buildings and public gathering places should be provided locations that reinforce community identity and support self-government.
- g. That Civic Buildings should be distinctive and appropriate to a role more important than the other buildings that constitute the fabric of the city.
- h. That the preservation and renewal of historic buildings should be facilitated to affirm the continuity and evolution of society.
- i. That the harmonious and orderly evolution of urban areas should be secured through graphic codes that serve as guides for change.

1.2 APPLICABILITY

- 1.2.1 Provisions of this Code are activated by "shall" when required; "should" when recommended; and "may" when optional.
- 1.2.2 This Code shall provide alternative land use and form-based regulations. The provisions of this Code, when in conflict, shall take precedence over those of other codes, ordinances, regulations and standards except the existing _____, _____ and _____ codes (the "Local Health and Safety Code").
- 1.2.3 The existing City of Albuquerque Zoning Ordinances and the City of Albuquerque Subdivision Ordinances (the "Existing Local Codes") shall continue to be applicable as the base zoning and development standards throughout the City, and in the I-25 Sector. Issues not covered in I-25 Form Code that would be in conflict with the Existing Local Codes Intent Section 1.2 shall be resolved in favor of the Existing Code.
- 1.2.4 Terms used throughout this Code shall be accorded their commonly accepted meanings or as defined in Articles 1-4 hereof or in the Definitions of Terms (Article 5). In the event of conflicts between these definitions and those of the Existing Local Codes, those of this Code shall take precedence.
- 1.2.5 The Definitions of Terms (Article 5) contains regulatory language that is integral to this Code. Capitalized terms in Articles 1-4 of this Code may refer to Article 5 Definitions.

2.1 INSTRUCTIONS

- 2.1.3 The I-25 Sector Plan shall identify, assign and follow the requirements of the Development Types described in Section 2.3.
- 2.1.4 The I-25 Sector Plan shall assign Transect Zones and Districts and Civic Functions within each Development Type as described in Sections 2.3 and 2.4 with detailed provisions for site and building development as described in Article 3.
- 2.1.5 I-25 Sector Plan shall consist of a Regulating Plan that shows the Transect Zones and Districts, and the second assigning the Special Requirements as provided in Section 2.4.

2.2 TRANSECT ZONES

- 2.2.1 Transect Zones shall be constituted of the elements described in Table 1 and the standards summarized in Table 14.

A transect, in its origins (Von Humboldt 1790), is a geographical crosssection of a region used to reveal a sequence of environments. Originally, it was used to analyze natural ecologies, showing varying characteristics through different zones such as shores, wetlands, plains, and uplands. For human environments, such a cross-section can be used to identify a set of habitats that vary by their level and intensity of urban character, a continuum that ranges from rural to urban.

In Transect planning, this range of environments is the basis for organizing the components of urbanization: building, lot, land use, street, and all of the other physical elements of the human habitat.

Transects are based on the selection and arrangement of all the components that contribute to a particular type of environment. Each environment, or Transect Zone (T-Zone), is comprised of elements that support and intensify its locational character. Through the Transect, planners are able to specify different urban contexts that have the function and intensity appropriate to their locations. For instance, a ranch house would undermine the immersive quality of a neighborhood center, whereas an apartment building would not. Wide roads and open swales find a place on the Transect in more rural areas, while narrow streets and raised curbs are appropriate for urban areas. Based on local practices, most elements can be locally calibrated to contribute to the regional character of a given place.

2.2.2 How the Transect Operates

- a. The zoning system uses the rural-to-urban Transect.
- b. The Transect is a framework that identifies a continuous range of habitats from the most natural to the most urban.
- c. The continuum of the Transect, when subdivided, lends itself to the creation of zoning categories.
- d. These zoning categories include standards that encourage diversity similar to that of organically evolved settlements.
- e. The standards specified by the zoning categories overlap, reflecting the successional ecozones of natural and human communities.
- f. The Transect integrates environmental and zoning methodologies, enabling environmentalists to assess the design of
- g. The standards specified by the zoning categories overlap, reflecting the successional ecozones of natural and human communities.
- h. The Transect integrates environmental and zoning methodologies, enabling environmentalists to assess the design of social habitats and urbanists to support the viability of natural ones.

2.3 DEVELOPMENT TYPES

The I-25 Sector Plan shall be planned according to the following Community Types. The I-25 Sector Plan shall include and assign, to the extent applicable: Neighborhoods and Corridors. I-25 Sector Plan shall be based on conserving, completing or creating Transect-based urban structure.

2.3.1 Neighborhoods (TND or Traditional Neighborhood Development)

Neighborhoods shall be urbanized areas at least 40 acres that are primarily Residential. A Neighborhood shall be based upon a partial or entire Standard Pedestrian Shed. The physical center of the Neighborhood should be located at an important traffic intersection associated with a Civic or Commercial institution. The edges of the Neighborhood should blend into an adjacent Neighborhood or Downtown without buffer.

2.3.2 Centers and Commercial Corridors (Center and Corridor Development)

- a. Centers shall be urbanized areas that are primarily mixed-use. A Center shall be defined by a Pedestrian Shed, oriented around an important Commercial Corridor. Centers should be the location of large Commercial and Retail uses as well as government and other Civic institutions of regional importance. The edges of a Center should blend into adjacent Districts and Neighborhoods without buffer.

- b. Commercial Corridors shall be primarily mixed-use, urbanized, lineal transportation thoroughfares. A commercial transportation corridor may be a lineal urban Transect Zone. A Commercial Corridor shall include Centers as nodal points or intersections defined by a Pedestrian Shed. The edges of a Corridors, like Centers should blend into adjacent Districts and Neighborhoods without buffer.

2.3.3 Heavy Manufacturing, Employment, and Balloon Park Districts (I-1, ED, BP)

- a. Special Districts for Heavy Manufacturing, Employment, and for the Balloon Park shall be areas dedicated for specific or special uses that by virtue of size or function, and cannot meet the requirements for any Transect Zone or combination of Zones.

2.4 DENSITY CALCULATIONS

- 2.4.1 The Developable Areas described in the Regulating Plan shall be considered the Gross Developable Area for purposes of density calculations, tabulated by Transect Zones and Districts in the Base Residential Densities and Other Functions specified in Table 11. These calculations reflect the intended, desired diversity of urbanism.
- 2.4.2 The Overall Density shall be calculated in terms of housing units as specified for the area of each Transect Zone by Table 11. For purposes of Density calculation, the Transect Zone Areas include the Thoroughfares but not land allocated to Civic Function.
- 2.4.3 The Overall Density shall be calculated first by referencing Table 11 (Summary Table), and Tables 8 and 9 (Building Function/Parking Calculation), and at the Density requirements at the Building Scale. Lodging, Office, or Retail areas shall be exchanged for housing units in making the Density calculations described in section 2.4.4.
- 2.4.4 The percent of the housing units shown on Table 11 shall be exchanged for other Functions at the following rates:
 - a. For Lodging: 2 bedrooms for each unit of Overall Density.
 - b. For Office or Retail: 500 square feet for each unit of Overall Density.
 - c. The number of units exchanged shall be approved by the City of Albuquerque.
- 2.4.5 The housing and other Functions for each Transect Zone shall be further adjusted at the building scale.

2.5 SPECIAL REQUIREMENTS

2.5.1 The I-25 Sector Plan may designate the following special requirements:

- a. A differentiation of the Thoroughfares as a Primary-Grid (P-Grid) and a Secondary-Grid (S-Grid). Buildings along the P-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the S-Grid may be more readily considered for automobile-oriented standards. The Frontages assigned to the S-Grid shall not exceed 30% of the total length within a Pedestrian Shed.
- b. A designation for mandatory or recommended Retail Frontage requiring that a building provide a shopfront at sidewalk level along the entire length of the Frontage. The shopfront shall be no less than 70% glazed in clear glass and provided with an awning overlapping the sidewalk as generally illustrated in Table 4. The first floor shall be confined to retail use through the depth of the First Layer.
- c. A designation for mandatory or recommended Gallery Frontage, requiring that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns. The Gallery Frontage may be combined with a Retail Frontage as shown in Table 3.
- d. A designation of Coordinated Streetscape Frontage, requiring that the Public and Private Frontages be coordinated as a single, coherent landscape and paving design.
- e. A designation for Cross Block Passages, requiring a minimum 8-foot-wide pedestrian access be reserved between certain buildings.

2.6 PRE-EXISTING CONDITIONS

- 2.6.1 Existing buildings that do not conform to the provisions of this Code may continue in use as they are until a Substantial Modification is requested, in accordance with City of Albuquerque ordinances and laws.
- 2.6.2 The modification of existing buildings is permitted by right if such changes result in greater conformance with the specifications of this section.

3.1 SPECIFIC TO GENERAL URBAN ZONES (T4)

The General Urban Zone consists of a mixed-use but primarily residential urban fabric dominated by and compatible with the existing single family housing. However, the zone includes a wide range of building types: single, sideyard, and rowhouses. Setbacks and landscaping are variable. Streets typically define medium-sized blocks.



3.1.1 Building Disposition (T4)

- a. Newly platted lots shall be dimensioned according to Section 3.1.8
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 3.1.8
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 12C.
- d. Lot coverage by building shall not exceed that shown in Section 3.1.8.
- e. Facades shall be built parallel to a rectilinear Principal Frontage Line or parallel to the tangent of a curved Principal Frontage Line.
- f. Setbacks for Principal Buildings shall be as shown in Table 11G. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by City Of Albuquerque.
- g. Rear Setbacks for Outbuildings shall be a minimum of 12 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 3.1.8.
- h. Building Types shall be as shown in Table 6.
- i. A minimum Residential housing mix of three Types (none less than 20%) shall be required in the General Urban Zone, selected from Table 6.



3.1.2 Building Configuration (T4)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 4 and Section 3.1.8.
- b. Awnings may encroach the public sidewalk without limit. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to 50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.
- c. Loading docks and service areas shall be permitted on Frontages only by City Of Albuquerque.
- d. Building Heights shall conform to Table 5 and be as shown in Section 3.1.8.
- e. All specified Building Heights may be increased by the base elevations required by applicable FEMA standards.

3.1.3 Building Function & Density (T4)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 7 or 8 and Section 3.1.8. Functions that do not conform to the requirements of Tables 7 or 8 shall require approval by City Of Albuquerque.
- b. The Actual Parking available to meet the Required Parking shown on Table 9 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraphs 3.1.3 c and 3.1.3 d.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 9). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. Within the Long Pedestrian Shed of a TOD, the Effective Parking available for calculating the intensity on each lot may be increased by a multiplier of thirty percent (30%).
- e. Accessory uses of Limited Lodging or Limited Office shall be permitted within an Outbuilding.



3.1.4 Parking Standards (T4)

- a. Vehicular parking shall be required as shown in Tables 8 and 9.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the City Of Albuquerque.
- d. Parking shall be accessed by the Alley or Rear Lane, when such are available.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 3.1.5b.
- f. All parking areas except for Driveways shall be located at the Third Layer as illustrated in Table 12D. Garages shall be at the Third Layer.
- g. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval.
- h. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.
- i. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by City Of Albuquerque (see Section x).



3.1.5 Architectural Standards (T4)

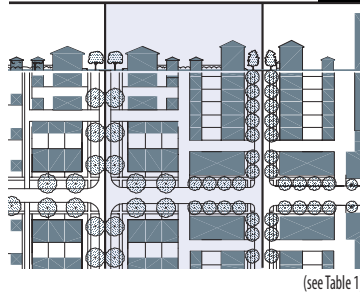
- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a fence by City Of Albuquerque. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total building wall area, with each Facade being calculated independently].
- e. Doors and windows that operate as sliders are prohibited along Frontages.
- f. Pitched roofs, if provided, shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12.
- g. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the City Of Albuquerque.
- h. The exterior finish material on all Facades shall be limited to brick, wood siding, fiber-cement siding and/or stucco.
- i. Balconies and porches shall be made of painted wood or metal.
- j. Fences, if provided within the First Lot Layer shall be painted. Fences at other Layers may be of wood board or chain link.

3.1.6 Environmental Standards (T4)

- a. The species of landscape installed shall consist primarily of durable species tolerant of soil compaction.
- b. Impermeable surface shall be confined to the ratio of lot coverage by building, as shown in Table 11F.

3.1.7 Landscape Standards (T4)

- a. Native plantings to approximate the number, type, and location of species of native plantings on the Public Frontage shall be planted within the First Layer of the Frontage Line as illustrated in Table 12D.
- b. Native plantings of species approximateing the planting on the Public Frontage as shown in Table 4. Native grasses shall be permitted.



(see Table 1)

BUILDING FUNCTION (see Tables 7 & 8)

a. Residential	open use
b. Lodging	open use
c. Office	limited use
d. Retail	open use
e. Civic	limited use
f. Employment	limited use
g. Industrial	

BUILDING HEIGHT (see Table 5)

a. Principal Building	3 stories max, 1 min
b. Outbuilding	2 stories max.

LOT OCCUPATION

a. Lot Width	18 ft min 96 ft max
b. Lot Coverage	70% max

BUILDING TYPE (see Table 6)

a. Edgeyard	permitted
b. Sideyard	permitted
c. Rearyard	permitted
d. Courtyard	
e. Specialized Employment	

BUILDING DISPOSITION

a. Front Setback	6 ft. min. 18 ft. max.
b. Side Setback	0 ft. combined min.
c. Rear Setback	3 ft. min.*
d. Frontage Buildout	

OUTBUILDING DISPOSITION

a. Front Setback	20 ft. min. + bldg. setback
b. Side Setback	0 ft. min. or 3 ft.
c. Rear Setback	3 ft.* or 23 ft.

PRIVATE FRONTAGES

a. Common Lawn	
b. Porch & Fence	permitted
c. Terrace or L.C.	permitted
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	

Refer to Summary Table 11

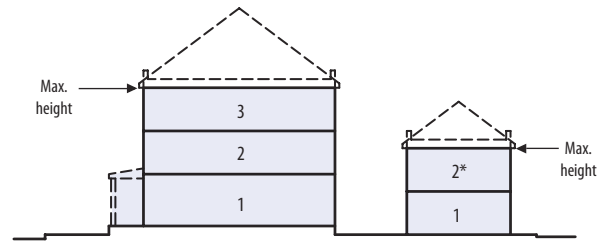
PARKING PROVISIONS

See Tables 8 & 9

* or 15 ft. from center line of alley

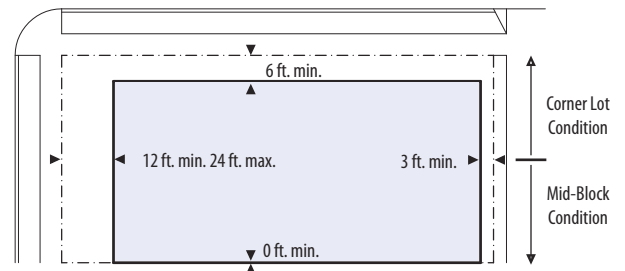
BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



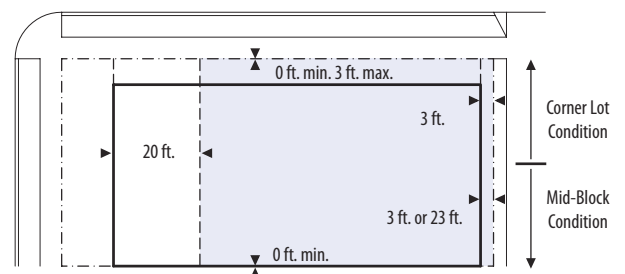
BUILDING DISPOSITION

1. The facades and elevations of principal buildings shall be distanced from the lot lines as shown.
2. Buildings shall have facades along principal frontage lines and elevations along lot lines. (see Table 12E).



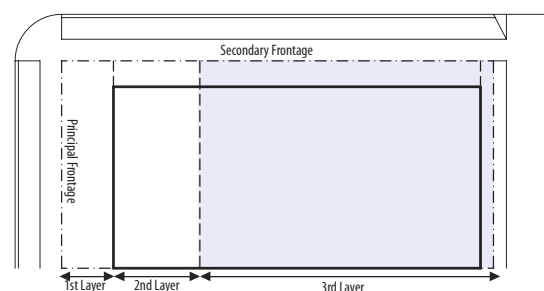
OUTBUILDING PLACEMENT

1. The elevations of the out buildings shall be distances from the lot lines as shown.



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 12D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 12D).
3. Trash containers shall be stored within the 3rd Layer.



3.2 SPECIFIC TO URBAN CENTER ZONES (T5)

The Urban Center Zone consists of higher density mixed-use building types that accommodate retail, offices, rowhouses and apartments. It has a tight network of streets, with wide sidewalks, steady street tree planting and buildings set close to the frontages.

3.2.1 Building Disposition (T5)

- a. Newly platted lots shall be dimensioned according to Section 3.2.8
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 3.2.8
- c. One principal building at the Frontage, and one outbuilding to the rear of the principal building, may be built on each lot as shown in Table 12C.
- d. Lot coverage by building shall not exceed that shown in Section 3.2.8.
- e. Facades shall be built parallel to the Principal Frontage Line along a minimum of 70% of its length on the Setback shown in Section 3.2.8. In the absence of a building along the remainder of the Frontage Line, a Streetscreen shall be built co-planar with the Facade.
- f. Setbacks for Principal Buildings shall be as shown in Table 12G. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by City Of Albuquerque.
- g. Rear Setbacks for Outbuildings shall be a minimum of 12 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 3.2.8.
- h. Building Types shall be as shown in Table 6.
- i. Buildings shall have their principal pedestrian entrances on a Frontage Line.

3.2.2 Building Configuration (T5)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 4 and Section 3.2.8.
- b. Awnings may encroach the public sidewalk without limit. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to 50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.
- c. Loading docks and service areas shall be permitted on Frontages only by the City Of Albuquerque.
- d. Building Heights shall conform to Table 5 and be as shown in Section 3.2.8.
- e. A first level Residential or Lodging Function shall be raised a minimum of 2 feet from average sidewalk grade.



3.2.3 Building Function & Density (T5)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 7 or 8 and Section 3.2.8. Functions that do not conform to the requirements of Tables 7 or 8 shall require approval by the City Of Albuquerque.
- b. The Actual Parking available to meet the Required Parking shown on Table 9 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraphs 3.2.3 c and 3.2.3 d.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 9). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. Within the Long Pedestrian Shed of a TOD, the Effective Parking available for calculating the intensity on each lot may be increased by a multiplier of thirty percent (30%).
- e. Accessory uses of Limited Lodging or Limited Office shall be permitted within an outbuilding.
- f. First story Commercial shall be permitted throughout and shall be required at Mandatory Shopfront Frontages.
- g. Manufacturing within the first Story may be permitted by the City Of Albuquerque.



3.2.4 Parking Standards (T5)

- a. Vehicular parking shall be required as shown in Tables 8 and 9.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the City Of Albuquerque.
- d. Parking shall be accessed by the Alley or Rear Lane when such is available.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 3.2.5b.
- f. All parking areas shall be located at the Third Lot Layer .
- g. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by City Of Albuquerque.



- h. The vehicular entrance of a parking lot or garage on a Frontage shall be no wider than 30 feet.
- i. Pedestrian entrances to all parking lots and parking structures shall be directly from a Frontage Line. Only underground parking structures may be entered by pedestrians directly from a Principal Building.
- j. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.
- k. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by the City Of Albuquerque (see Section X).



3.2.5 Architectural Standards (T5)

- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a fence by City Of Albuquerque. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total building wall area, with each Facade being calculated independently.
- e. The Facades on Retail Frontages shall be detailed as storefronts and glazed with clear glass no less than 70% of the sidewalk-level story.
- f. Doors and windows that operate as sliders are prohibited along Frontages.
- g. Buildings may have flat roofs enclosed by parapets or sloped roofs. Pitched roofs shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12.
- h. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the City Of Albuquerque.
- i. The exterior finish materials on all Facades shall be limited to stone, brick and/or stucco.
- j. Balconies, galleries and arcades shall be made of concrete, painted wood or metal.
- k. Streetscreens shall be located coplanar with the building Facade line as shown in Table 12D.

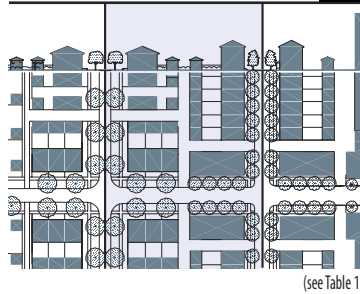


3.2.6 Environmental Standards (T5)

- a. The landscape installed shall consist primarily of durable species tolerant of soil compaction.
- b. Impermeable surface by building shall be confined to the ratio of lot coverage as shown in Table 11F.

3.2.7 Landscape Standards (T5)

- a. Native plantings to approximate the number, type, and location of species of native plantings on the Public Frontage shall be planted within the First Layer of the Frontage Line as illustrated in Table 12D.
- b. The First Layer as shown in Table 12D shall be landscaped or paved to approximate the enfronting Public Frontage as shown in Table 4.
- c. Native plantings shall be a species that, either through canopy sufficient height, transparency, or configuration, do not visually block the storefronts and signage of commercial frontages .



BUILDING FUNCTION (see Tables 7 & 8)

a. Residential	open use
b. Lodging	open use
c. Office	open use
d. Retail	open use
e. Civic	limited use
f. Employment	open use
g. Industrial	limited use

BUILDING HEIGHT (see Table 5)

a. Principal Building	4 stories max. 2 min.
b. Outbuilding	2 stories max.

LOT OCCUPATION

a. Lot Width	18 ft min 180 ft max
b. Lot Coverage	80% max

BUILDING TYPE (see Table 6)

a. Edgeyard	
b. Sideyard	permitted
c. Rearyard	permitted
d. Courtyard	permitted
e. Specialized Employment	

BUILDING DISPOSITION

a. Front Setback	0 ft. min. 12 ft. max.
b. Side Setback	0 ft. min. 24 ft. max.
c. Rear Setback	3 ft. min.*
d. Frontage Buildout	70% min at setback

OUTBUILDING DISPOSITION

a. Front Setback	40 ft. max. from rear prop.
b. Side Setback	0 ft. min.*
c. Rear Setback	3 ft. max.

PRIVATE FRONTAGES

a. Common Lawn	
b. Porch & Fence	
c. Terrace or L.C.	permitted
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	permitted

Refer to Summary Table 11

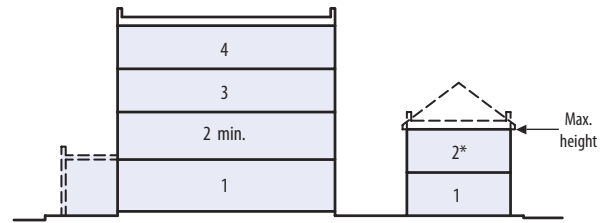
PARKING PROVISIONS

See Tables 8 & 9

* or 15 ft. from center line of alley

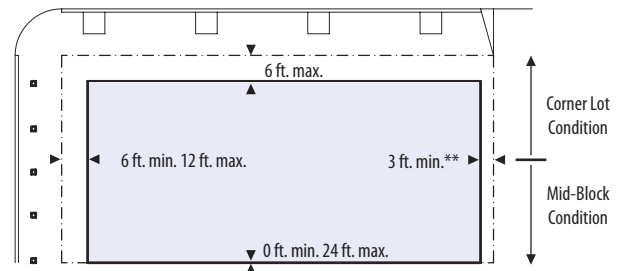
BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



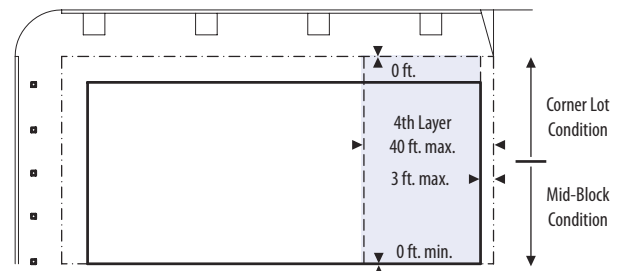
BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
2. Buildings shall have facades along the principal frontage lines and elevations along lot lines (see Table 12E).



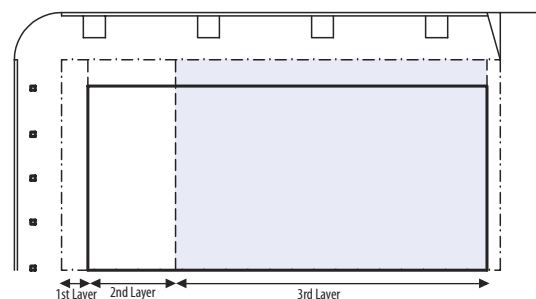
OUTBUILDING DISPOSITION

1. The elevations of the out buildings shall be distances from the lot lines as shown.



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 12D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 12D).
3. Trash containers shall be stored within the 3rd Layer as shown in the diagram (see Table 12D).



3.3 SPECIFIC TO PRIMARY URBAN CORRIDOR ZONES (T6-A)

The Primary Urban Corridor Zone consists of the highest density. It contains the tallest buildings, the greatest variety, and unique civic buildings, and is the least naturalistic.



3.3.1 Building Disposition (T6-A)

- a. Newly platted lots shall be dimensioned according to Section 3.3.8.
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 3.3.8.
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 12C.
- d. Lot coverage by building shall not exceed that shown in Section 3.3.8.
- e. Facades shall be built parallel to the Principal Frontage Line along a minimum of 80% of its length on the Setback shown in Section 3.3.8. In the absence of building along the remainder of the Frontage Line, a streetscreen shall be built co-planar with the Facade.
- f. Setbacks for Principal Buildings shall be as shown in Table 12G. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by City Of Albuquerque.
- g. Rear Setbacks for Outbuildings shall be a minimum of 12 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 3.3.8.
- h. Building Types shall be as shown in Table 6.
- i. Buildings shall have their principal pedestrian entrances on a Frontage Line.



3.3.2 Building Configuration (T6-A)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 4 and Section 3.3.8.
- b. Awnings may encroach the public sidewalk without limit. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to 50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.
- c. Loading docks and service areas shall be permitted on Frontages only by the City Of Albuquerque.



- d. Building Heights shall conform to Table 5 and be allocated as required in Section 3.3.8.
- e. A first level Residential Function or Lodging Function shall be raised a minimum of 2 feet from average sidewalk grade.

3.3.3 Building Function & Density (T6-A)

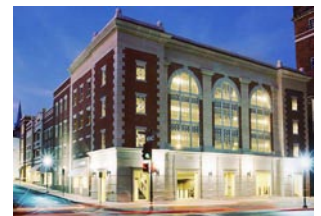
- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 7 or 8 and Section 3.3.8. Functions that do not conform to the requirements of Tables 7 or 8 shall require approval by the City Of Albuquerque.
- b. The Actual Parking available to meet the Required Parking shown on Table 9 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraphs 3.3.3c and 3.3.3d.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 9). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. Within the Long Pedestrian Shed of a TOD, the Effective Parking available for calculating the intensity on each lot may be increased by a multiplier of thirty percent (30%).
- e. Ground floor Commercial shall be permitted throughout and shall be required at Mandatory Shopfront Frontages.
- f. Manufacturing within the first story shall be permitted by the City Of Albuquerque.



Park-It-On-Market

3.3.4 Parking Standards (T6-A)

- a. Vehicular parking shall be required as shown in Tables 8 and 9.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the City Of Albuquerque.
- d. Parking shall be accessed by the Alley or Rear Lane, when such are available.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 3.3.5b.



- f. All parking areas shall be located at the Third Lot Layer.
- g. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by City Of Albuquerque.
- h. The vehicular entrance of a parking lot or garage on a Frontage shall be no wider than 30 feet.
- i. Pedestrian entrances to all parking lots and parking structures shall be directly from a Frontage Line. Only underground parking structures may be entered by pedestrians directly from a Principal Building.
- j. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.
- k. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by the City Of Albuquerque (see Section X).

3.3.5 Architectural Standards (T6-A)

- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a fence by the City Of Albuquerque. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total building wall area, with each Facade being calculated independently.
- e. The Facades on Retail Frontages shall be detailed as storefronts and glazed with clear glass no less than 70% of the sidewalk-level story.
- f. Doors and windows that operate as sliders are prohibited along Frontages.
- g. Buildings may have flat roofs enclosed by parapets, or sloped roofs. Pitched roofs shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:15.
- h. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the City Of Albuquerque.
- i. Streetscreens shall be located coplanar with the Facades as shown in Table 12C.

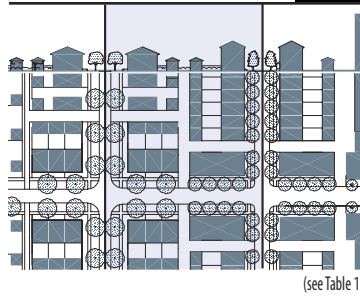


3.3.6 Environmental Standards (T6-A)

- a. The species of landscape installed shall consist primarily of durable species tolerant of soil compaction.
- b. Impermeable surface by building shall be confined to the ratio of lot coverage by building as shown in Table 11F.

3.3.7 Landscape Standards (T6-A)

- a. Native plantings to approximate the number, type, and location of species of native plantings on the Public Frontage shall be planted within the First Layer of the Frontage Line as illustrated in Table 12D.
- b. The First Layer shall be landscaped or paved to approximate the Public Frontage as shown in Table 3.
- c. Native plantings shall be a species that, either through canopy sufficient height, transparency, or configuration, do not visually block the storefronts and signage of commercial frontages .
- d. Native plantings shall not be required in the First Layer (Table 12D).
- e. Outdoor storage shall be screened from view from any Frontage by a native planting screen or visual architectural screen in conformance with Section 3.3.5b.



BUILDING FUNCTION (see Tables 7 & 8)

a. Residential	open use
b. Lodging	open use
c. Office	open use
d. Retail	open use
e. Civic	limited use
f. Employment	open use
g. Industrial	open use

BUILDING HEIGHT (see Table 5)

a. Principal Building	12 stories max. 4 min.
b. Outbuilding	N/A

LOT OCCUPATION

a. Lot Width	18 ft. min 700 ft. max
b. Lot Coverage	90% max

BUILDING TYPE (see Table 6)

a. Edgeyard	
b. Sideyard	
c. Rearyard	permitted
d. Courtyard	permitted
e. Specialized Employment	

BUILDING DISPOSITION

a. Front Setback	0 ft. min. 12 ft. max.
b. Side Setback	0 ft. min. 24 ft. max.
c. Rear Setback	0 ft. min.
d. Frontage Buildout	80% min. at setback

OUTBUILDING DISPOSITION

a. Front	N/A
b. Side	N/A
c. Rear	N/A

PRIVATE FRONTAGES

a. Common Lawn	
b. Porch & Fence	
c. Terrace or L.C.	
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	permitted

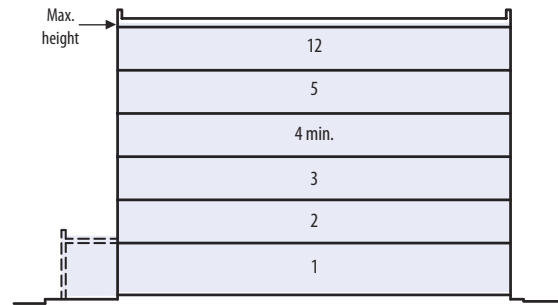
Refer to Summary Table 11

PARKING PROVISIONS

See Tables 8 & 9

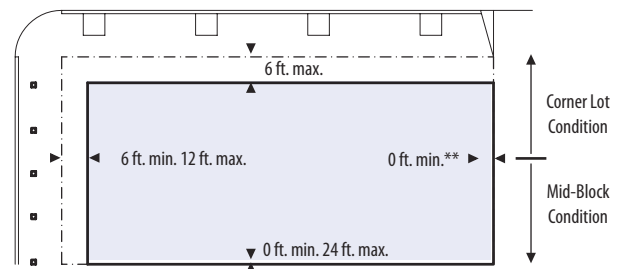
BUILDING HEIGHT

1. Building height shall be measured in number of stories, not including a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



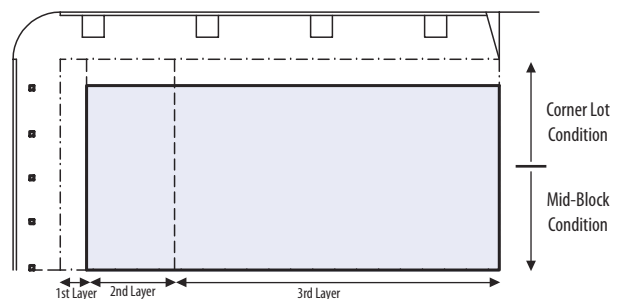
BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
2. Buildings shall have facades along frontage lines and elevations along lot lines (see Table 12E).



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 12D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 12D).
3. Trash containers shall be stored within the 3rd Layer as shown in the diagram (see Table 12D).



3.4 SPECIFIC TO SECONDARY URBAN CORRIDOR TRANSECT ZONES (T6-B)

The Urban Corridor Zone consists of densities higher than the T5 zones but less than the T6-A zones. It contains a range and variety of buildings, and unique civic buildings.



3.4.1 Building Disposition (T6-B)

- a. Newly platted lots shall be dimensioned according to Section 3.4.8.
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 3.4.8.
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 12C.
- d. Lot coverage by building shall not exceed that shown in Section 3.4.8.
- e. Facades shall be built parallel to the Principal Frontage Line along a minimum of 75% of its length on the Setback shown in Section 3.4.8. In the absence of building along the remainder of the Frontage Line, a streetscreen shall be built co-planar with the Facade.
- f. Setbacks for Principal Buildings shall be as shown in Table 12G. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by the City Of Albuquerque.
- g. Rear Setbacks for Outbuildings shall be a minimum of 12 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 3.4.8.
- h. Building Types shall be as shown in Table 6.
- i. Buildings shall have their principal pedestrian entrances on a Frontage Line.



3.4.2 Building Configuration (T6-B)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 4 and Section 3.4.8.
- b. Awnings may encroach the public sidewalk without limit. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to 50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.
- c. Loading docks and service areas shall be permitted on Frontages only by the City Of Albuquerque.



- d. Building Heights shall conform to Table 5 and be allocated as required in Section 3.4.8.
- e. A first level Residential Function or Lodging Function shall be raised a minimum of 2 feet from average sidewalk grade.

3.4.3 Building Function & Density (T6-B)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 7 or 8 and Section 3.4.8. Functions that do not conform to the requirements of Tables 7 or 8 shall require approval by the the City Of Albuquerque.
- b. The Actual Parking available to meet the Required Parking shown on Table 9 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraphs 3.4.3c and 3.4.3d.
- c. The Base Density may be adjusted upward by adding the Actual Parking available or each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 9). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. Within the Long Pedestrian Shed of a TOD, the Effective Parking available for calculating the intensity on each lot may be increased by a multiplier of thirty percent (30%).
- e. Ground floor Commercial shall be permitted throughout and shall be required at Mandatory Shopfront Frontages.
- f. Manufacturing within the first story shall be permitted by the City Of Albuquerque.

3.4.4 Parking Standards (T6-B)

- a. Vehicular parking shall be required as shown in Tables 8 and 9.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the City Of Albuquerque.
- d. Parking shall be accessed by the Alley or Rear Lane, when such are available.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 3.4.5b.

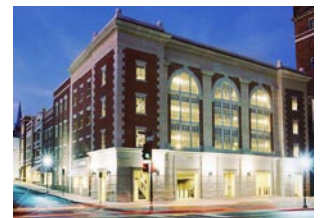
- f. All parking areas shall be located at the Third Lot Layer.
- g. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by City Of Albuquerque.
- h. The vehicular entrance of a parking lot or garage on a Frontage shall be no wider than 30 feet.
- i. Pedestrian entrances to all parking lots and parking structures shall be directly from a Frontage Line. Only underground parking structures may be entered by pedestrians directly from a Principal Building.
- j. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.
- k. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by the City Of Albuquerque (see Section X).

3.4.5 Architectural Standards (T6-B)

- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a fence by the City Of Albuquerque. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total building wall area, with each Facade being calculated independently.
- e. The Facades on Retail Frontages shall be detailed as storefronts and glazed with clear glass no less than 70% of the sidewalk-level story.
- f. Doors and windows that operate as sliders are prohibited along Frontages.
- g. Buildings may have flat roofs enclosed by parapets, or sloped roofs. Pitched roofs shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:15.
- h. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the City Of Albuquerque.
- i. Streetscreens shall be located coplanar with the Facades as shown in Table 12C.



Park-It-On-Market



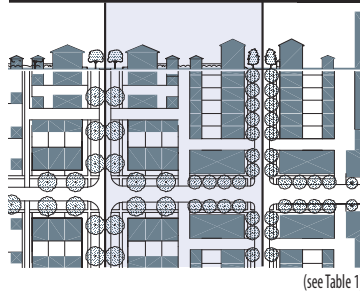
3.4.6 Environmental Standards (T6-B)

- a. The species of landscape installed shall consist primarily of durable species tolerant of soil compaction.
- b. Impermeable surface by building shall be confined to the ratio of lot coverage by building as shown in Table 11F.

3.4.7 Landscape Standards (T6-B)

- a. Native plantings to approximate the number, type, and location of species of native plantings on the Public Frontage shall be planted within the First Layer of the Frontage Line as illustrated in Table 12D.
- b. The First Layer shall be landscaped or paved to approximate the Public Frontage as shown in Table 3.
- c. Native plantings shall be a species that, either through canopy sufficient height, transparency, or configuration, do not visually block the storefronts and signage of commercial frontages.
- d. Native plantings shall not be required in the First Layer (Table 12D).
- e. Outdoor storage shall be screened from view from any Frontage by a native planting screen or visual architectural screen in conformance with Section 3.3.5b.





BUILDING FUNCTION (see Tables 7 & 8)

a. Residential	open use
b. Lodging	open use
c. Office	open use
d. Retail	open use
e. Civic	limited use
f. Employment	open use
g. Industrial	open use

BUILDING HEIGHT (see Table 5)

a. Principal Building	8 stories max. 2 min.
b. Outbuilding	N/A

LOT OCCUPATION

a. Lot Width	18 ft. min 700 ft. max
b. Lot Coverage	80% max

BUILDING TYPE (see Table 6)

a. Edgeyard	
b. Sideyard	
c. Rearyard	permitted
d. Courtyard	permitted
e. Specialized Employment	

BUILDING DISPOSITION

a. Front Setback	0 ft. min. 12 ft. max.
b. Side Setback	0 ft. min. 24 ft. max.
c. Rear Setback	0 ft. min.
d. Frontage Buildout	75% min. at setback

OUTBUILDING DISPOSITION

a. Front	N/A
b. Side	N/A
c. Rear	N/A

PRIVATE FRONTAGES

a. Common Lawn	
b. Porch & Fence	
c. Terrace or L.C.	
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	permitted

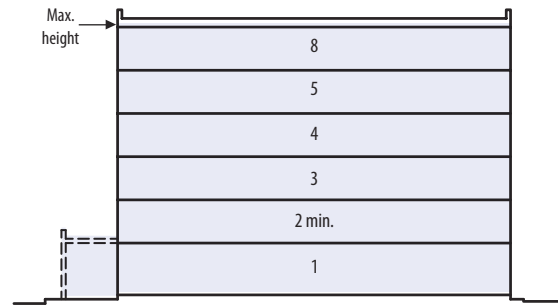
Refer to Summary Table 11

PARKING PROVISIONS

See Tables 8 & 9

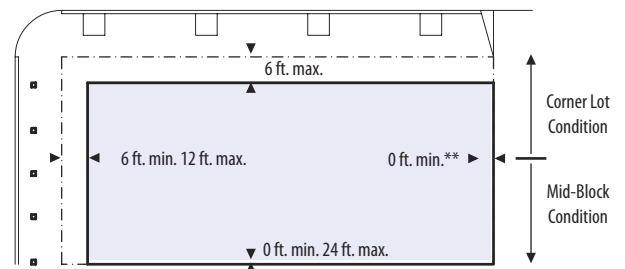
BUILDING HEIGHT

1. Building height shall be measured in number of stories, not including a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



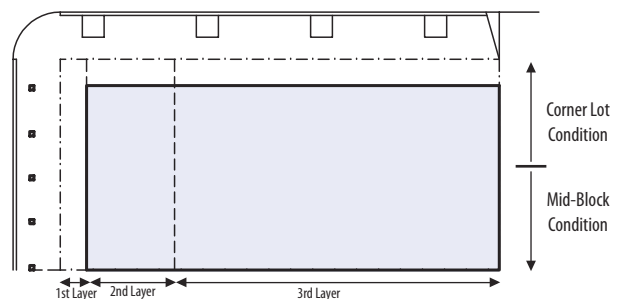
BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
2. Buildings shall have facades along frontage lines and elevations along lot lines (see Table 12E).



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 12D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 12D).
3. Trash containers shall be stored within the 3rd Layer as shown in the diagram (see Table 12D).



3.5 SPECIFIC TO EMPLOYMENT TRANSITION TRANSECT ZONES (ET)

The Urban Employment Transition Zone consists of single or mixed-use building types that accommodate retail, offices, and light industrial uses, and provides a transition between T-4/5 and ED or Employment Districts consisting of landscape buffers along the transitional edges of adjacent property lines.

3.5.1 Building Disposition (ET)

- a. Newly platted lots shall be dimensioned according to Section 3.5.8
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 3.5.8
- c. One principal building at the Frontage, and one outbuilding to the rear of the principal building, may be built on each lot as shown in Table 12C.
- d. Lot coverage by building shall not exceed that shown in Section 3.5.8.
- e. Facades shall be built parallel to the Principal Frontage Line along a minimum of 50% of its length on the Setback shown in Section 3.5.8.
- f. Setbacks for Principal Buildings shall be as shown in Table 12G. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by City Of Albuquerque.
- g. Rear Setbacks for Outbuildings shall be a minimum of 12 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 3.5.8
- h. Rear Setbacks for Acoustical/Visual Walls shall be a maximum of 5 feet measured Rear Lane easement.
- i. Building Types shall be as shown in Table 6.
- j. Buildings shall have their principal pedestrian entrances on a Frontage Line.



3.5.2 Building Configuration (ET)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 4 and Section 3.5.8.
- b. Awnings may encroach the public sidewalk without limit. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to 50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.
- c. Loading docks and service areas shall be permitted on Frontages only by the City Of Albuquerque.
- d. Building Heights shall conform to Table 5 and be as shown in Section 3.5.8.



3.5.3 Building Function & Density (ET)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 7 or 8 and Section 3.5.8. Functions that do not conform to the requirements of Tables 7 or 8 shall require approval by the City Of Albuquerque.
- b. The Actual Parking available to meet the Required Parking shown on Table 9 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraphs 3.5.3c and 3.5.3 d.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 9). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.
- d. Within the Long Pedestrian Shed of a TOD, the Effective Parking available for calculating the intensity on each lot may be increased by a multiplier of thirty percent (30%).
- e. First story Commercial shall be permitted throughout and shall be required at Mandatory Shopfront Frontages.
- f. Manufacturing within the first Story may be permitted by the City Of Albuquerque.

3.5.4 Parking Standards (ET)

- a. Vehicular parking shall be required as shown in Tables 8 and 9.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the City Of Albuquerque.
- d. Parking shall be accessed by the Alley or Rear Lane when such is available.
- e. Parking lots shall be masked from the Frontage by a Liner Building or Streetscreen as specified in Section 3.5.5b.
- f. All parking areas shall be located at the Third Lot Layer .
- g. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by City Of Albuquerque.
- h. The vehicular entrance of a parking lot or garage on a Frontage shall be no wider than 30 feet.

- i. Pedestrian entrances to all parking lots and parking structures shall be directly from a Frontage Line. Only underground parking structures may be entered by pedestrians directly from a Principal Building.
- j. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every ten vehicular parking spaces.
- k. For buildings on Secondary Grids (S-Grids), parking lots may be allowed on the Frontage by the City Of Albuquerque (see Section X).

3.5.5 Architectural Standards (ET)

- a. On the frontage facade, building wall materials may only be combined horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a fence by City Of Albuquerque. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings on the frontage facade, including porches, galleries, arcades and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. The Facades on Retail Frontages shall be detailed as storefronts and glazed with clear glass no less than 65% of the sidewalk-level story.
- e. Doors and windows that operate as sliders are prohibited along Frontages.
- f. Buildings may have flat roofs enclosed by parapets or sloped roofs. Pitched roofs shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12.
- g. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment to the satisfaction of the City Of Albuquerque.
- h. Streetscreens shall be located coplanar with the building Facade line as shown in Table 12D.

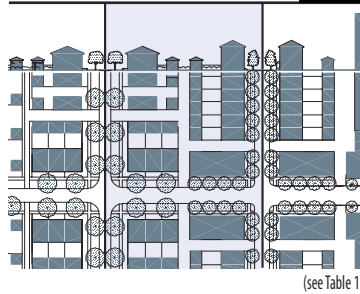


3.5.6 Environmental Standards (ET)

- a. The landscape installed shall consist primarily of durable species tolerant of soil compaction.
- b. Impermeable surface by building shall be confined to the ratio of lot coverage as shown in Table 11F.

3.5.7 Landscape Standards (ET)

- a. A visual and acoustical barrier shall be located along or within 5 feet of the rear and/or side property line(s) adjacent to T4/5 zones, to separate T4/5 zones from Employment Districts.
- b. The visual and acoustical barrier shall consist of a solid wall between 6 and 8 feet in height and constructed of a material approximateing the adjacent building Facade with a minimum 50 STC acoustical value, or a reinforced acoustical fabric wall between 6 and 8 feet in height with the minimum 60 STC acoustical value, and faced along the T4/5 side with native plantings or other landscaping materials that screen at least 90% of the wall.
- c. Native plantings to approximate the number, type, and location of species of native plantings on the Public Frontage shall be planted within the First Layer of the Frontage Line as illustrated in Table 12D.
- d. The First Layer as shown in Table 12D shall be landscaped or paved to approximate the enfronting Public Frontage as shown in Table 3.



BUILDING FUNCTION (see Tables 7 & 8)

a. Residential	limited use
b. Lodging	
c. Office	open use
d. Retail	open use
e. Civic	limited use
f. Employment	limited use
g. Industrial	open use

BUILDING HEIGHT (see Table 5)

a. Principal Building	4 stories max. 1 min.
b. Outbuilding	No greater than principal

LOT OCCUPATION

a. Lot Width	18 ft min 180 ft max
b. Lot Coverage	80% max

BUILDING TYPE (see Table 6)

a. Edgeyard	
b. Sideyard	permitted
c. Rearyard	permitted
d. Courtyard	permitted
e. Specialized Employment	

BUILDING DISPOSITION

a. Front Setback	0 ft. min. 12 ft. max.
b. Side Setback	0 ft. min. 24 ft. max.
c. Rear Setback	3 ft. min.*
d. Frontage Buildout	65% min at setback

OUTBUILDING DISPOSITION

a. Front Setback	40 ft. max. from rear prop.
b. Side Setback	0 ft. min.*
c. Rear Setback	3 ft. max.

PRIVATE FRONTAGES

a. Common Lawn	
b. Porch & Fence	
c. Terrace or L.C.	permitted
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	permitted

Refer to Summary Table 11

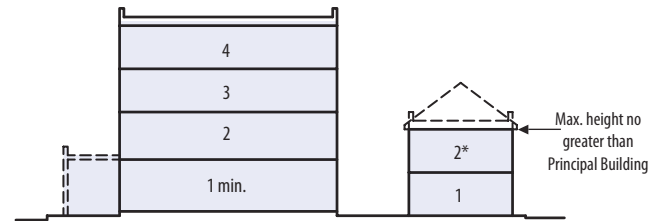
PARKING PROVISIONS

See Tables 8 & 9

* or 15 ft. from center line of alley

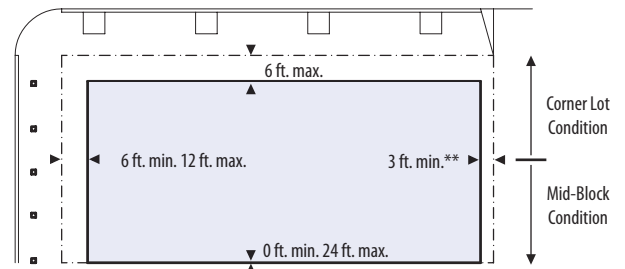
BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



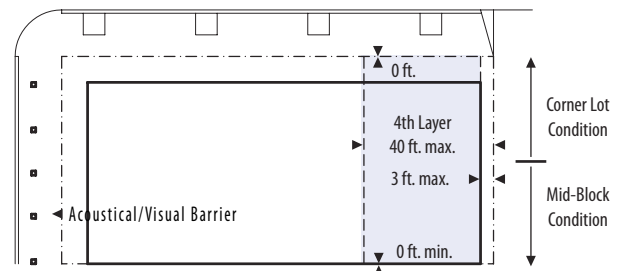
BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
2. Buildings shall have facades along the principal frontage lines and elevations along lot lines (see Table 12E).



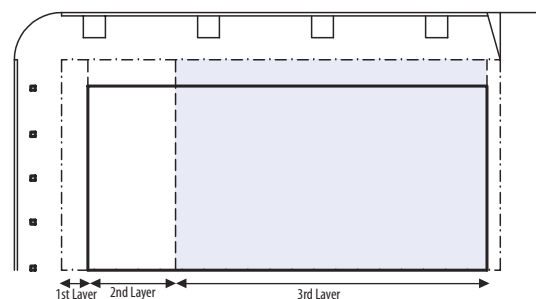
OUTBUILDING AND ACOUSTICAL/VISUAL BARRIER DISPOSITION

1. The elevations of the out buildings shall be distances from the lot lines as shown.
2. The acoustical/visual barrier shall be located along the property line of the T4/S zone.



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 12D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 12D).
3. Trash containers shall be stored within the 3rd Layer as shown in the diagram (see Table 12D).



3.6 SPECIFIC TO THE EMPLOYMENT DISTRICT ZONE:

3.6.1 Building Disposition (ED)

- a. Newly platted lots shall be dimensioned according to Section 3.6.8.
- b. Buildings shall be disposed in relation to the boundaries of their lots according to Section 3.6.8.
- c. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot as shown in Table 12C.
- d. Lot coverage by building shall not exceed that shown in Section 3.6.8.
- e. Setbacks for Principal Buildings shall be as shown in Table 12G. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by City Of Albuquerque.
- f. Rear Setbacks for Outbuildings shall be a minimum of 12 feet measured from the centerline of the Alley or Rear Lane easement. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Section 3.6.8.
- g. Building Types shall be as shown in Table 6.
- h. Buildings shall have their principal pedestrian entrances on a Frontage Line.

3.6.2 Building Configuration (ED)

- a. Private Frontage types shall conform to and be allocated in accordance with Table 4 and Section 3.6.8.
- d. Building Heights shall conform to Table 5 and be allocated as required in Section 3.6.8.
- c. A first level Residential Function or Lodging Function shall be raised a minimum of 2 feet from average sidewalk grade.

3.6.3 Building Function & Density (ED)

- a. Buildings in each Transect Zone shall conform to the Functions described in Tables 7 or 8 and Section 3.6.8. Functions that do not conform to the requirements of Tables 7 or 8 shall require approval by the City Of Albuquerque.
- b. Base Density in the ED District shall be calculated as 1 employee per 200 square feet of building for office use, and 1 employee per 500 square feet of building for manufacturing or other industrial uses, or as determined by the City Of Albuquerque.

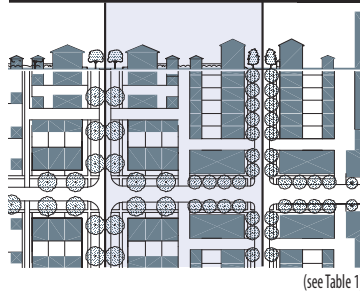
- c. The Actual Parking available to meet the Required Parking shown on Table 9 shall constitute the Base Density. Functions shall be limited by the Base Density, subject to upward adjustment in accordance with paragraphs 3.6.3c and 3.6.3d.
- d. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor (Table 9). The result shall be the Effective Parking available for calculating an Adjusted Density. Conversely: The Effective Parking required is the sum of the Required Parking divided by the Sharing Factor.

3.6.4 Parking Standards (ED)

- a. Vehicular parking shall be required as shown in Tables 8 and 9.
- b. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.
- c. Maximum Parking ratios may be established by the City Of Albuquerque.
- d. Parking shall be accessed by the Alley or Rear Lane, when such are available.
- e. Parking lots shall be masked from the Frontage by a Streetscreen or Landscaping as specified in Section 3.6.5b.
- f. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by City Of Albuquerque.

SECTION 3.6.8

ED



BUILDING FUNCTION (see Tables 7 & 8)

a. Residential	limited use
b. Lodging	
c. Office	
d. Retail	open use
e. Civic	limited use
f. Employment	limited use
g. Industrial	open use

BUILDING HEIGHT (see Table 5)

a. Principal Building	6 stories max. 1 min.
b. Outbuilding	N/A

LOT OCCUPATION

a. Lot Width	100 ft. min no max
b. Lot Coverage	80% max

BUILDING TYPE (see Table 6)

a. Edgeyard	
b. Sideyard	
c. Rearyard	permitted
d. Courtyard	permitted
e. Specialized Employment	permitted

BUILDING DISPOSITION

a. Front Setback	12 ft. min.
b. Side Setback	24 ft. min.
c. Rear Setback	24 ft. min.
d. Frontage Buildout	none required

OUTBUILDING DISPOSITION

a. Front	N/A
b. Side	N/A
c. Rear	N/A

PRIVATE FRONTAGES

a. Common Lawn	
b. Porch & Fence	
c. Terrace or L.C.	
d. Forecourt	permitted
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	permitted
h. Arcade	permitted

Refer to Summary Table 11

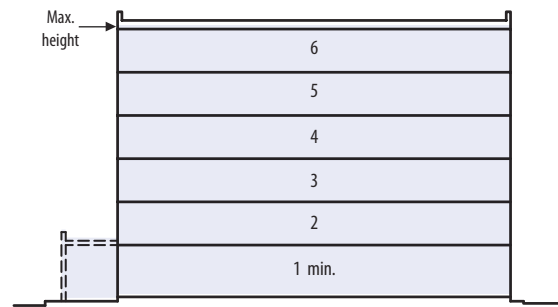
PARKING PROVISIONS

See Tables 8 & 9

EMPLOYMENT DISTRICT

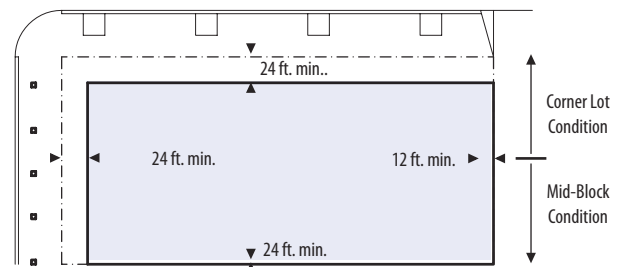
BUILDING HEIGHT

1. Building height shall be measured in number of stories, not including a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the site.

3.7 CIVIC FUNCTIONS

3.7.1 General

- a. I-25 Sector Plan shall require and/or designate Civic Space (CS), including jurisdictional and institutional buildings, and Civic Buildings (CB), including parks, playgrounds, plazas, and squares.
- b. Civic Functions shall be required in all Transect Zones, but shall not be required in Employment Districts.
- d. Civic Functions shall not occupy more than a total of 20% of the area, nor less than 10% of the gross land area within its zone.
- c. Parking for Civic Functions shall be determined by the City of Albuquerque.



3.7.2 Civic Space (CS)

- a. Civic Spaces may be approved by the City of Albuquerque in any Transect Zone.
- b. Civic Spaces shall be generally designed as described in Table 10.

3.7.3 Civic Building (CB)

- a. Civic Buildings shall be approved by the City of Albuquerque within each Transect Zone on sites designated for Civic Buildings.
- b. Civic Buildings shall not be subject to the Requirements of Article 3. The particulars of their design shall be determined by the City of Albuquerque.



3.8 SPECIAL REQUIREMENTS

3.8.1 A The Sector Plan may designate the following special requirements:

- a. A differentiation of the Thoroughfares as a Primary-Grid (P-Grid) and a Secondary-Grid (S-Grid). Buildings along the P-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the S-Grid may be more readily considered for the City Of Albuquerque's allowing automobile-oriented standards. The Frontages assigned to the S-Grid shall not exceed 30% of the total length within a Pedestrian Shed.
- b. A designation for Mandatory or Recommended Retail Frontage requiring that a building provide a Shopfront at sidewalk level along the entire length of the Frontage. The Shopfront shall be no less than 70% glazed in clear glass and provided with an awning overlapping the sidewalk as generally illustrated in Table 4. The first floor shall be confined to Retail use through the depth of the First Layer.

- c. A designation for mandatory or recommended Gallery Frontage, requiring that a building provide a permanent cover over the sidewalk, either cantilevered or supported by columns. The Gallery Frontage may be combined with a Retail Frontage as shown in Table 4.
- d. A designation of Coordinated Streetscape Frontage, requiring that the Public and Private Frontages be coordinated as a single, coherent landscape and paving design.
- e. A designation of Terminated Vista location, requiring that the building be provided with architectural articulation of a type and character that responds to the location as approved by the City Of Albuquerque.
- f. A designation for Cross Block Passages, requiring a minimum 8-foot-wide pedestrian access be reserved between buildings.

TABLE 1: Transect Zone Descriptions. This table provides description of the character of each Transect Zone.

T4	<p>THE GENERAL URBAN ZONE consists of a mixed-use but primarily residential urban fabric dominated by and compatible with the existing single family housing. However, the zone includes a wide range of building types: single, sideyard, and rowhouses. Setbacks and landscaping are variable. Streets typically define medium-sized blocks.</p>	
T5	<p>THE URBAN CENTER ZONE consists of higher density mixed-use building types that accommodate retail, offices, rowhouses and apartments. It has a tight network of streets, with wide sidewalks, steady street tree planting and buildings set close to the frontages.</p>	
T6-A	<p>THE PRIMARY URBAN CORRIDOR ZONE consists of the highest density. It contains the tallest buildings, the greatest variety, and unique civic buildings. It is the least naturalistic; street plantings are steadily planted and sometimes absent.</p>	
T6-B	<p>THE SECONDARY URBAN CORRIDOR ZONE is identical consists of higher density and taller buildings, and a greater variety than T5, but not as intense as T6-A. It is less naturalistic than T5; street plantings are steadily planted and sometimes absent.</p>	
ET	<p>THE URBAN EMPLOYMENT TRANSITION ZONE consists of single or mixed-use building types that accommodate retail, offices, and light industrial uses, and provides a transition between T-4/5 and ED or Employment Districts consisting of landscape buffers along the transitional edges of adjacent property lines.</p>	
ED	<p>THE EMPLOYMENT DISTRICT consists of a primarily single use zone that accommodates light-to-heavy industrial uses that require physical separation from adjacent T4 through T6 zones and a physical separation from public rights-of-way including streets and roadways because of environmental, security and/or safety concerns. Office, retail, offices, and other functions are permitted as complementary uses and a limited amount of live/work and work/live building types are allowed.</p>	

TABLE 2A: Vehicular Lane Dimensions. This table assigns lane widths to Transect Zones. The Design ADT (Average Daily Traffic) is the determinant for each of these sections. The most typical assemblies are shown in Table 3B. Specific requirements for truck and transit bus routes and truck loading shall be decided by City Of Albuquerque.

DESIGN SPEED	TRAVEL LANE WIDTH	T4	6.1	T6-A	T6-B	ET	ED
Below 20 mph	8 feet	<input type="checkbox"/>					
20-25 mph	9 feet	■	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	■
25-35 mph	10 feet	■	■	■	■	■	■
25-35 mph	11 feet		■	■	■	■	■
Above 35 mph	12 feet		■	■	■	■	■

DESIGN SPEED	PARKING LANE WIDTH	T4	6.1	T6-A	T6-B	ET	ED
20-25 mph	(Angle) 18 feet		■	■	■	■	■
20-25 mph	(Parallel) 7 feet	■					
25-35 mph	(Parallel) 8 feet	■	■	■	■	■	■
Above 35 mph	(Parallel) 9 feet		■	■	■	■	■

DESIGN SPEED	EFFECTIVE TURNING RADIUS	T4	6.1	T6-A	T6-B	ET	ED
Below 20 mph	5-10 feet	■	■		■		
20-25 mph	10-15 feet	■	■	■	■	■	■
25-35 mph	15-20 feet	■	■	■	■	■	■
Above 35 mph	20-30 feet		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

■ BY RIGHT

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TABLE 2B: Vehicular Lane/Parking Assemblies. The projected design speeds determine the dimensions of the vehicular lanes and turning radii assembled to create thoroughfares.

	ONE WAY MOVEMENT		TWO WAY MOVEMENT	
a. NO PARKING Design AADT Pedestrian Crossing Design Speed				T6-A T6-B 22,000 VPD 9 Seconds 36,000 VPD 13 Seconds 35 MPH and above
b. YIELD PARKING Design ADT Pedestrian Crossing	T4 1,000 VPD 5 Seconds		T4 1,000 VPD 7 Seconds	
c. PARKING ONE SIDE PARALLEL Design ADT Pedestrian Crossing Design Speed	T4 5,000 VPD 5 Seconds 20-30 MPH	T4 T5 18,000 VPD 8 Seconds	T5 16,000 VPD 8 Seconds 25-30 MPH	T5 T6-A T6-B 15,000 VPD 11 Seconds 25-30 MPH
d. PARKING BOTH SIDES PARALLEL Design ADT Pedestrian Crossing Design Speed	T4 8,000 VPD 7 Seconds Below 20 MPH	T4 T5 T6-B 20,000 VPD 10 Seconds 25-30 MPH	T4 T5 T6-A T6-B 15,000 VPD 10 Seconds 25-30 MPH	T5 T6-A T6-B 22,000 VPD 13 Seconds 25-30 MPH
e. PARKING BOTH SIDES DIAGONAL Design ADT Pedestrian Crossing Design Speed	T5 T6-B 18,000 VPD 15 Seconds Below 20 MPH	T5 T6-B 20,000 VPD 17 Seconds 20-25 MPH	T5 T6-A T6-B 15,000 VPD 17 Seconds 20-25 MPH	T5 T6-A T6-B 22,000 VPD 20 Seconds 25-30 MPH
f. PARKING ACCESS Design ADT Pedestrian Crossing Design Speed			T4 4 Seconds	T5 T6-A T6-B 8 Seconds

TABLE 3: Public Frontages

The **Public Frontage** is the area between the private lot line and the edge of the vehicular lanes.

	PLAN	
	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE
a. (EDR) For Employment District Roads : This frontage has open swales drained by percolation, foot and bicycle trails and no parking. The landscaping consists of the natural condition or multiple species arrayed in naturalistic clusters. Buildings are buffered by distance or berms.		ED
b. (SR) For Standard Roads : This frontage has open swales drained by percolation and a walking path or bicycle trail along one or both sides and parking. The landscaping consists of multiple species arrayed in naturalistic clusters.		T4 ET ED
c. (RS) For Residential Street : This frontage has raised curbs drained by inlets and narrow sidewalks separated from the vehicular lanes by a wide continuous planter, with parking on one or both sides. The landscaping consists of native plantings of a single or alternating species aligned in a regularly spaced alley.		T4 T5 ET
d. (SS) (AV) For Standard Streets or Avenues : This frontage has raised curbs drained by inlets and wide sidewalks separated from the vehicular lanes by a narrow continuous planter with parking on both sides. The landscaping consists of a single tree species aligned in a regularly spaced alley.		T5 T6-A T6-B
e. (CS) (AV) For Commercial Streets or Avenues : This frontage has raised curbs drained by inlets and very wide sidewalks along both sides separated from the vehicular lanes by separate tree wells with grates and parking on both sides. The landscaping consists of a single tree species aligned with regular spacing where possible but clears the shopfront entrances.		T5 T6-A T6-B

TABLE 4: Private Frontages. The Private Frontage is the area between the building and the lot lines.

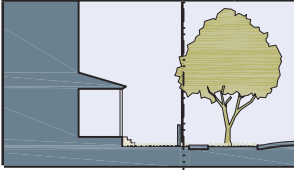
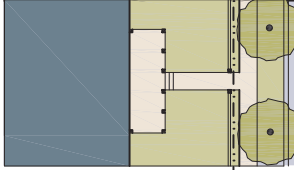


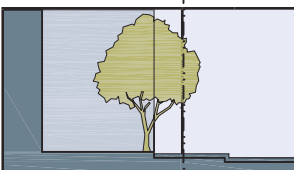
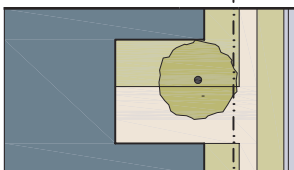
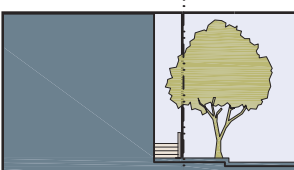

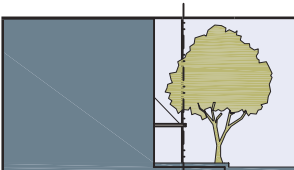
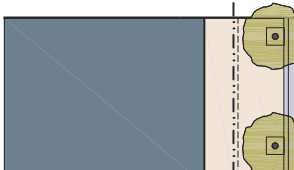
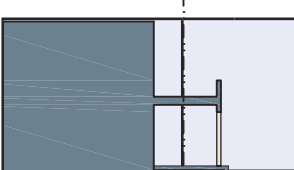
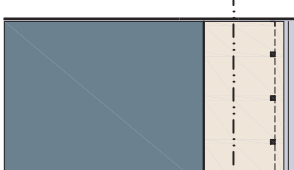
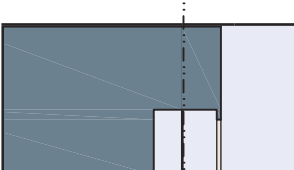

	SECTION		PLAN	
	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE	LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE
a. Porch & Fence: a frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroaching. A fence at the frontage line maintains the demarcation of the yard. The porches shall be no less than 8 feet deep.			T4	
b. Terrace or Light Court: a frontage wherein the facade is set back from the frontage line by an elevated terrace or a sunken light court. This type buffers residential use from urban sidewalks and removes the private yard from public encroachment. The terrace is suitable for conversion to outdoor cafes.			T4 T5	
c. Forecourt: a frontage wherein a portion of the facade is close to the frontage line and the central portion is set back. The forecourt created is suitable for vehicular drop-offs. This type should be allocated in conjunction with other frontage types.			T4 T5 T6	
d. Stoop: a frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.			T4 T5 T6	
e. Shopfront and Awning: a frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level and an awning that may overlap the sidewalk to the maximum extent possible.			T4 T5 T6	
f. Gallery: a frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. This type is conventional for retail use. The gallery shall be no less than 10 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb.			T4 T5 T6	
g. Arcade: a frontage wherein the facade is a colonnade that overlaps the sidewalk, while the facade at sidewalk level remains at the frontage line. This type is conventional for retail use. The arcade shall be no less than 12 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb.			T5 T6	

TABLE 5: Building Configuration. This table shows prescribed building heights for each Transect Zone. The vertical extent of a building is measured by number of stories, not including a raised basement or an inhabited attic. Heights are measured from the average grade of the frontage line to the eave of a pitched roof or to the surface of a flat roof. The term “tower” here refers to an attached or detached addition to a building, not to an entire building.

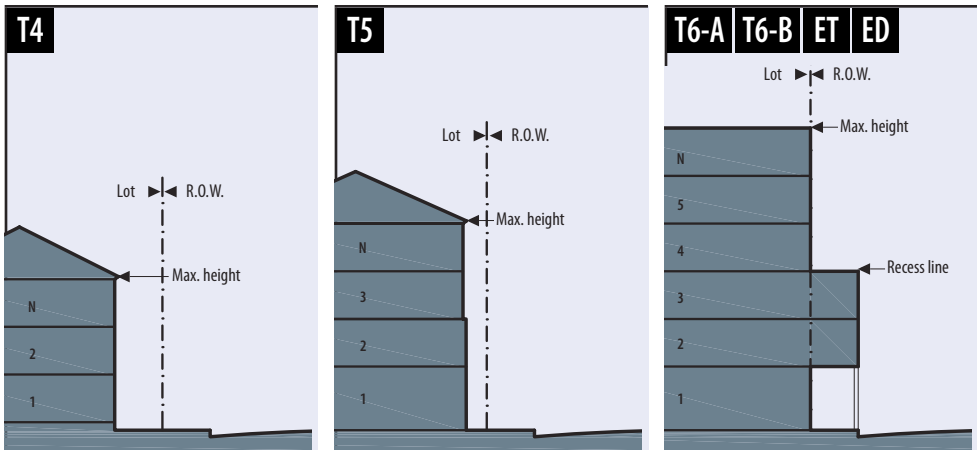


TABLE 6: Building Type. This table approximates the location of the structure relative to the boundaries of each individual lot, establishing suitable basic building types for each Transect Zone.

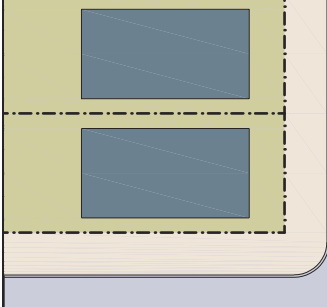


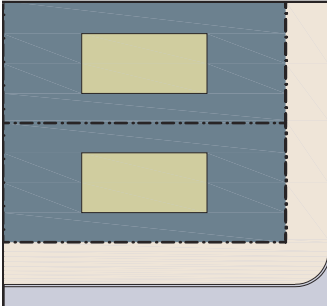
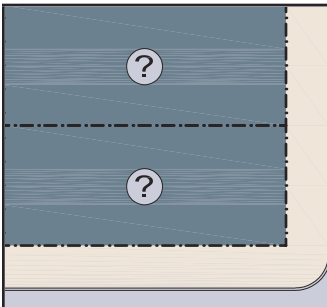
<p>a. Edgeyard: Specific Types - Single family House, Cottage, Villa, Estate House, Urban Villa. A building that occupies the center of its lot with Setbacks on all sides. This is the least urban of types as the front yard sets it back from the frontage, while the side yards weaken the spatial definition of the public Thoroughfare space. The front yard is intended to be visually continuous with the yards of adjacent buildings. The rear yard can be secured for privacy by fences and a well-placed Backbuilding and/or Outbuilding.</p>	 <div>T4</div>
<p>b. Sideyard: Specific Types - Charleston Single House, zero-lot-line house. A building that occupies one side of the lot with the Setback to the other side. The visual opening of the side yard on the street frontage causes this building type to appear freestanding. A shallow frontage Setback defines a more urban condition. If the adjacent building is similar with a blank party wall, the yard can be quite private. This type permits systematic climatic orientation in response to the sun or the breeze.</p>	 <div>T4 T5 ET</div>
<p>c. Rearyard: Specific Types - Townhouse, Rowhouse, Live-Work unit, perimeter block. A building that occupies the full frontage, leaving the rear of the lot as the sole yard. This is a very urban type as the continuous Facade steadily defines the public Thoroughfare. The rear Elevations may be articulated for functional purposes. In its Residential form, this type is the Rowhouse. For its Commercial form, the rear yard can accommodate substantial parking.</p>	 <div>T4 T5 T6-A T6-B ET ED</div>
<p>d. Courtyard: Specific Types - Patio House. A building that occupies the boundaries of its lot while internally defining one or more private patios. This is the most urban of types, as it is able to shield the private realm from all sides while strongly defining the public Thoroughfare. Because of its ability to accommodate incompatible activities, masking them from all sides, it is recommended for workshops, Lodging and schools. The high security provided by the continuous enclosure is useful for crime-prone areas.</p>	 <div>T5 T6-A T6-B ET ED</div>
<p>e. Specialized Employment: A building that is not subject to categorization. Buildings dedicated to manufacturing, storage, and transportation.</p>	 <div>ED</div>

TABLE 7: Specific Function. This table expands the Building Function categories of Table 8 to delegate specific functions within Transect Zones. Table 7 should be customized for local character and requirements.

	T4	T5	T6-A	T6-B	ET	ED
a. RESIDENTIAL						
Apartment Building	■	■	■	■		
Row House	■	■				
Duplex House	■	■				
Sideyard House	■	■				
Cottage	■					
House	■					
Estate House						
Accessory Unit	■	■				
Manufactured House						
Temporary Tent	□	□	□	□		
Live-Work Unit	■	■	■	■	■	□
b. LODGING						
Hotel (no room limit)		■	■	■		□
Inn (up to 12 rooms)	■	■	■	■	■	
Inn (up to 5 rooms)	■	■	■	■	■	
S.R.O. hostel	□	□	□	□	□	
School Dormitory	■	■	■	■		
c. OFFICE						
Office Building	■	■	■	■	■	■
Live-Work Unit	■	■	■	■	□	□
d. RETAIL						
Open-Market Building	■	■	■	■	■	
Retail Building	■	■	■	■	■	■
Display Gallery	■	■	■	■		
Restaurant	■	■	■	■	■	
Kiosk	■	■	■	■		
Push Cart		□	□	□		
Liquor Selling Establishment		□	□	□		
Adult Entertainment			□	□		
e. CIVIC						
Bus Shelter	■	■	■	■	■	■
Convention Center			□	□		
Conference Center		□	■	■		
Exhibition Center			□	□		
Fountain or Public Art	■	■	■	■	■	
Library	■	■	■	■		
Live Theater		■	■	■		
Movie Theater		■	■	■		
Museum		□	■	■		
Outdoor Auditorium		■	■	■		
Parking Structure		■	■	■	■	■
Passenger Terminal		□	□	□		
Playground	■	■	■	■	□	
Sports Stadium			□	□		
Surface Parking Lot	■	□	□	□	■	■
Religious Assembly		■	■	■	□	

	T4	T5	T6-A	T6-B	ET	ED
f. OTHER: AGRICULTURE						
Grain Storage						■
Livestock Pen						□
Greenhouse					■	■
Stable					□	□
Kennel	□	□	□	□	□	□
f. OTHER: AUTOMOTIVE						
Gasoline		□	□	□	□	■
Automobile Service					□	■
Truck Maintenance					□	■
Drive -Through Facility		□	□	□	□	■
Rest Stop						□
Roadside Stand					■	
Billboard			□	□		□
Shopping Center						
Shopping Mall						
f. OTHER: CIVIL SUPPORT						
Fire Station	■	■	■	■	■	□
Police Station	■	■	■	■	■	□
Cemetery	□				■	□
Funeral Home	■	■	■	■	□	
Hospital		□	□	□		
Medical Clinic	□	■	■	■		
f. OTHER: EDUCATION						
College		□	□	□		
High School	□	□	□	□		
Trade School		□	□	□		
Elementary School	■	■	■	■	□	
Other- Childcare Center	■	■	■	■	■	
f. OTHER: INDUSTRIAL						
Heavy Industrial Facility						■
Light Industrial Facility			□	□	■	■
Truck Depot					□	■
Laboratory Facility			□	□	□	■
Water Supply Facility					□	■
Sewer and Waste Facility						■
Electric Substation	□	□	□	□	□	■
Wireless Transmitter					□	■
Cremation Facility						■
Warehouse			□	□	□	■
Produce Storage					□	■
Mini-Storage					□	■

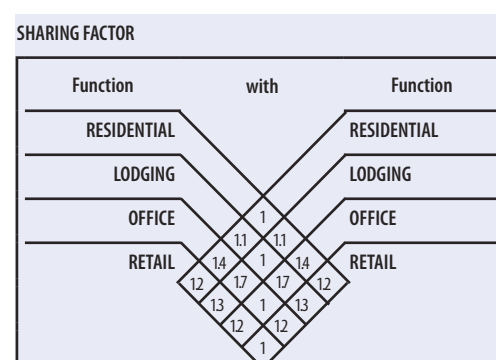
■ BY RIGHT
□ BY City Of Albuquerque



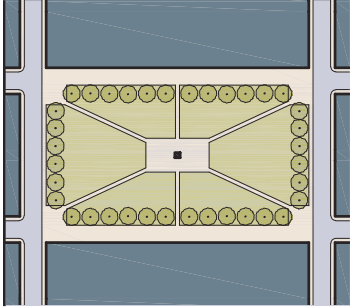
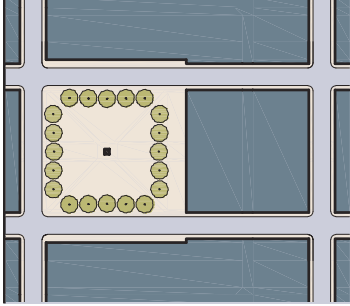
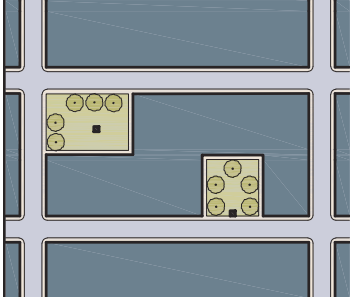
TABLE 8: Building Function - General. This table categorizes building functions within Transect Zones. For greater precision describing the functions, see Table 7.

	ED	ET	T4	T5	T6-A	T6-B
a. RESIDENTIAL			Limited Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling, a ratio which may be reduced according to the shared parking standards (See Table 10).	Open Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling, a ratio which may be reduced according to the shared parking standards (See Table 10).		
b. LODGING	Limited Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to twelve, in addition to the parking requirement for the dwelling. Food service may be provided in the a.m. The maximum length of stay shall not exceed ten days.		Limited Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to twelve, in addition to the parking requirement for the dwelling. Food service may be provided in the a.m. The maximum length of stay shall not exceed ten days.	Open Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom. Food service may be provided at all times.		
c. OFFICE	Open Office: The building area available for office use on each lot is limited by the requirement of 2.0 assigned parking places per 1000 square feet of net office space.		Limited Office: The building area available for office use on each lot is limited to the first story of the principal building and/or to the ancillary building, and by the requirement of 3.0 assigned parking places per 1000 square feet of net office space in addition to the parking requirement for each dwelling.	Open Office: The building area available for office use on each lot is limited by the requirement of 2.0 assigned parking places per 1000 square feet of net office space.		
d. RETAIL	Open Retail: The building area available for retail use is limited by the requirement of 3.0 assigned parking places per 1000 square feet of net retail space.		Limited Retail: The building area available for retail use is limited to the first story of buildings at corner locations, not more than one per block, and by the requirement of 4.0 assigned parking places per 1000 square feet of net retail space in addition to the parking requirement of each dwelling. The specific use shall be further limited to neighborhood store, or food service seating no more than 40.	Open Retail: The building area available for retail use is limited by the requirement of 3.0 assigned parking places per 1000 square feet of net retail space.		
e. CIVIC	See Table 10		See Table 10	See Table 10		
f. OTHER	See Table 10		See Table 10	See Table 10		

TABLE 9: Parking Calculation. The Required Parking table summarizes the parking requirements of Table 9 for each site or, conversely, the amount of building allowed on each site given the parking available.

REQUIRED PARKING (See table 8)						
	ED	ET	T4	T5	T6-A	T6-B
RESIDENTIAL			1.5 / dwelling	1.0 / dwelling		
LODGING		1.0 / bedroom	1.0 / bedroom	1.0 / bedroom		
OFFICE		3.0 / 1000 sq. ft.	3.0 / 1000 sq. ft.	2.0 / 1000 sq. ft.		
RETAIL		4.0 / 1000 sq. ft.	4.0 / 1000 sq. ft.	3.0 / 1000 sq. ft.		
CIVIC	To be determined by City Of Albuquerque					
EMPLOYMENT	To be determined by City Of Albuquerque					
INDUSTRIAL	To be determined by City Of Albuquerque					



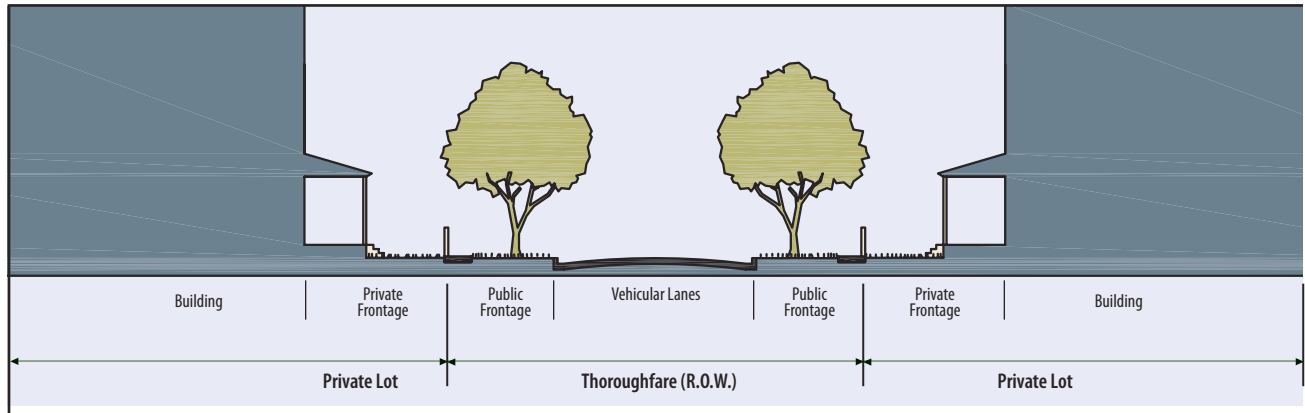
<p>a. Park: A natural preserve available for unstructured recreation. A park may be independent of surrounding building frontages. Its landscape shall consist of paths and trails, meadows, woodland and open shelters, all naturalistically disposed. Parks may be lineal, following the trajectories of natural corridors. The minimum size shall be 15 acres. Larger parks may be approved by City Of Albuquerque as districts in all zones.</p>	 <div data-bbox="1360 247 1399 340"> <p>T4</p> <p>ET</p> </div>
<p>b. Green: An open space, available for unstructured recreation. A green may be spatially defined by landscaping rather than building frontages. Its landscape shall consist of lawn and native plantings, naturalistically disposed. The minimum size shall be 2 acres and the maximum shall be 15 acres.</p>	 <div data-bbox="1360 562 1399 609"> <p>T4</p> </div>
<p>c. Square: An open space available for unstructured recreation and civic purposes. A square is spatially defined by building frontages. Its landscape shall consist of paths, lawns and native plantings, formally disposed. Squares shall be located at the intersection of important thoroughfares. The minimum size shall be 1 acre and the maximum shall be 5 acres.</p>	 <div data-bbox="1360 886 1399 1029"> <p>T4</p> <p>T5</p> <p>T6-A</p> </div>
<p>d. Plaza: An open space, available for civic purposes and commercial activities. A plaza shall be spatially defined by building frontages. Its landscape shall consist primarily of pavement. Trees are optional. Plazas shall be located at the intersection of important streets. The minimum size shall be 1 acre and the maximum shall be 2 acres.</p>	 <div data-bbox="1360 1213 1399 1398"> <p>T5</p> <p>T6-A</p> <p>T6-B</p> <p>ET</p> </div>
<p>e. Playground: An open space designed and equipped for the recreation of children. A playground shall be fenced and may include an open shelter. Playgrounds shall be interspersed within residential areas and may be placed within a block. Playgrounds may be included within parks and greens. There shall be no minimum or maximum size.</p>	 <div data-bbox="1360 1537 1399 1768"> <p>T4</p> <p>T5</p> <p>T6-A</p> <p>T6-B</p> <p>ET</p> </div>

Note: All requirements in this Table are subject to calibration for local context.

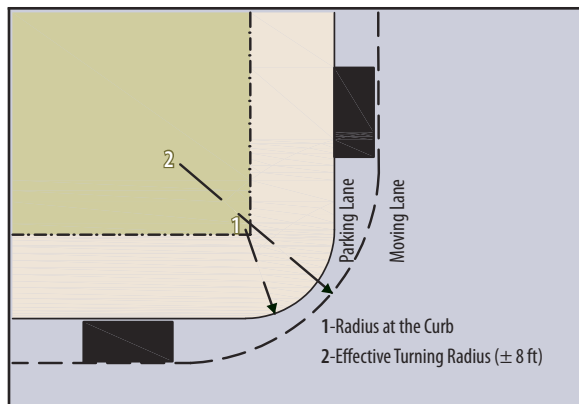
LESS URBAN				MORE URBAN			
T4 URBAN GENERAL				T5 URBAN CENTER			
T6-A PRIMARY CORRIDOR				T6-B SECONDARY CORRIDOR			
ET EMPLOYMENT TRANSITION				ED EMPLOYMENT DISTRICT			

A. BASE RESIDENTIAL DENSITY						
By Right	6 units / acre gross	12 units / acre gross	48 units / acre gross	32 units / acre gross	2 units / acre gross	2 units / acre gross
Other Functions	20 - 30% min	30 - 50% min	50 - 70% min	50 - 70% min	no minimum	no minimum
B. BLOCK SIZE						
Block Perimeter	2400 ft. max	2200 ft. max	3000 ft. max	2600 ft. max	2600 ft. max	no maximum
C. PUBLIC FRONTS (see Table 3)						
EDR						permitted
SR						permitted
RS						
SS & AV		permitted				
CS & AV		permitted				
D. CIVIC SPACES (see Table 11)						
Park	permitted					permitted
Green	permitted					
Square	permitted					
Plaza		permitted				
Playground	permitted					
E. LOT OCCUPATION						
Lot Width	18 ft. min 96 ft. max	18 ft. min 180 ft. max	18 ft. min 700 ft. max	18 ft. min 700 ft. max	18 ft. min 180 ft. max	100 ft. min
Lot Coverage	70% max	80% max	90% max	80% max	80% max	80% max
F. BUILDING DISPOSITION						
Front Setback	6 ft. min 18 ft. max	0 ft. min 12 ft. max	0 ft. min 12 ft. max	0 ft. min 12 ft. max	12 ft. min	12 ft. min
Side Setback	0 ft. combined min	0 ft. min 24 ft. max	0 ft. min 24 ft. max	0 ft. min 24 ft. max	24 ft. min	24 ft. min
Rear Setback	3 ft. min *	3 ft. min *	0 ft. min	0 ft. min	24 ft. min	24 ft. min
Frontage Buildout		70% min at setback	80% min at setback	75% min at setback		
G. BUILDING TYPE (see Table 6)						
Edgeyard	permitted					
Sideyard	permitted				permitted	
Rearyard	permitted					
Courtyard		permitted				
Specialized Employment						permitted
H. PRIVATE FRONTS (see Table 7)						
Common Yard						
Porch & Fence	permitted					
Terrace or L.C.	permitted				permitted	
Forecourt	permitted					
Stoop	permitted					
Shopfront & Awning	permitted					
Gallery	permitted					
Arcade		permitted				
I. BUILDING HEIGHT (see Table 8)						
Principal Building	3 stories max, 1 min	4 stories max, 2 min	12 stories max, 4 min	8 stories max, 2 min	4 stories max, 1 min	6 stories max, 1 min
Outbuilding	2 stories max	2 stories max	not applicable	not applicable	no greater than principal	not applicable
J. BUILDING FUNCTION (see Table 10 & 11)						
Residential	open use				limited use	
Lodging	open use					
Office	limited use	open use				
Retail	open use					
Civic	limited use					
Employment	limited use	open use				
Industrial		limited use	open use			

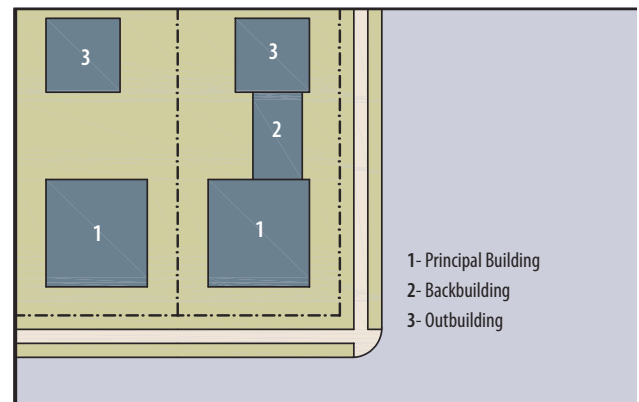
a. THOROUGHFARE & FRONTAGES



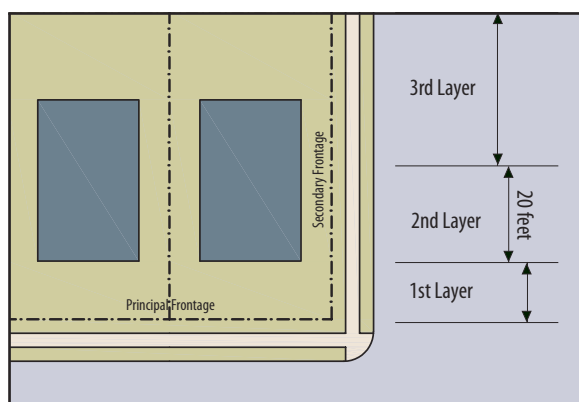
b. TURNING RADIUS



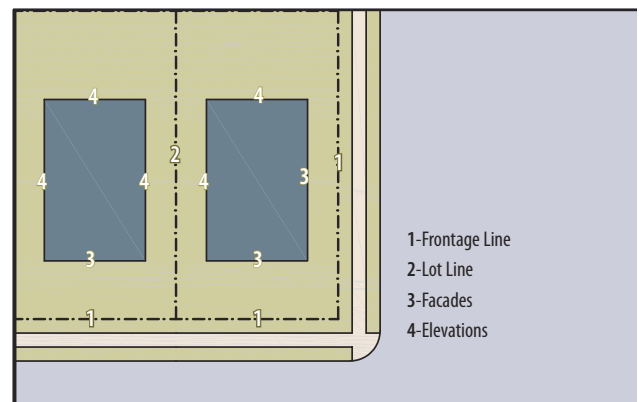
c. BUILDING DISPOSITION



d. LOT LAYERS



e. FRONTAGE & LOT LINES



This Article provides definitions for terms in this Code that are technical in nature or that otherwise may not reflect a common usage of the term. If a term is not defined in this Article, then the Planning Office shall determine the correct definition of the term.

DEFINITIONS

Affordable Housing: dwellings consisting of rental units or for-sale units. Both shall be economically within the means of the equivalent of the starting salary of a local elementary school teacher.

Allee: a regularly spaced and aligned row of trees usually planted along a Thoroughfare or Pedestrian Path.

Ancillary Unit: an apartment not greater than 600 square feet sharing ownership and utility connections with a Principal Building. An Ancillary Unit may or may not be within an outbuilding. Ancillary Units do not count toward maximum density calculations (see Tables 16 and 12).

Apartment: a dwelling unit sharing a building and a lot with other dwellings and/or uses. Apartments may be for rent or for sale as condominiums.

Avenue (AV): a thoroughfare of high vehicular capacity and low speed. Avenues are short distance connectors between urban centers. Avenues may be equipped with a landscaped median. Avenues become collectors upon exiting urban areas.

Backbuilding: a single-story structure connecting a principal building to an outbuilding (see Table 16).

Bicycle Lane (BL): a dedicated bicycle lane running within a moderate-speed vehicular thoroughfare, demarcated by striping.

Bicycle Route (BR): a thoroughfare suitable for the shared use of bicycles and automobiles moving at low speeds.

Bicycle Trail (BT): a bicycle way running independently of a high-speed vehicular thoroughfare.

Block: the aggregate of private lots, passages, rear lanes and alleys, circumscribed by thoroughfares.

Block Face: the aggregate of all the building facades on one side of a block. The Block Face provides the context for establishing architectural harmony.

Boulevard (BV): a thoroughfare designed for high vehicular capacity and moderate speed. Boulevards are long-distance thoroughfares traversing urbanized areas. Boulevards are usually equipped with slip roads buffering sidewalks and buildings. Boulevards become arterials upon exiting urban areas.

Brownfield: an area previously used primarily as an industrial site.

Building Configuration: the form of a building, based on its massing, private frontage, and height.

Building Disposition: the placement of a building on its lot (see Tables 9 & 16).

Building Function: the uses accommodated by a building and its lot. Functions are categorized as Restricted, Limited, or Open, according to the intensity of the use (see Tables 10 & 11).

Building Height: the vertical extent of a building measured in stories, not including a raised basement or a habitable attic. Height limits do not apply to masts, belfries, clock towers, chimney flues, water tanks, elevator bulkheads and similar structures. Building Height shall be measured from the average grade of the enfronting thoroughfare (see Table 8).

Building Type: a structure category determined by function, disposition on the lot, and configuration, including frontage and height.

By Right Permit: a proposal for a building or community plan that complies with this code and may thereby be processed administratively, without public hearing (see **Deviations**).

CLD: Conservation Land Development, Clustered Land Development. An incomplete neighborhood, standing free in the countryside. Because of a location away from transportation, CLD has a weak commercial center. This is the only Community type permitted by Right in the Restricted Growth Sector. (Syn: Hamlet, Cluster)

Civic: the term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit, and municipal parking.

Civic Building: a building designed specifically for a civic function. Civic Buildings shall not be subject to the requirements of Article 5. The particulars of their design shall be determined by City Of Albuquerque.

Civic Parking Reserve: parking structure or lot within a quarter-mile of the site that it serves. Space may be leased or bought from this Reserve to satisfy parking requirements.

Civic Space: an outdoor area dedicated for public use. Civic Space types are defined by the combination of certain physical constants including the relationship between their intended use, their size, their landscaping and their enfronting buildings (see Table 13).

Commercial: the term collectively defining workplace, office and retail functions.

Common Destination: An area of focused community activity defining the approximate center of a Pedestrian Shed. It may include without limitation one or more of the following: a Civic Space, a Civic Building, a Commercial center, a bus stop. A Common Destination may act as the social center of a Neighborhood. (See Section 3.7 Civic Function).

Community Type: a category defining the physical form of a settlement. The three basic Community Types addressed in this Code are CLD, TND, and TOD/TOD. The choice of Community Type will depend upon the regional Sector, level of urban intensity desired, particulars of the site, transportation, and implementation.

Context: surroundings made up of the particular combination of elements that create specific habitat.

Corridor: a lineal geographic system incorporating transportation and/or greenway trajectories. A transportation corridor may be a lineal urban Transect Zone. A commercial or mixed use center along a transit corridor, a major intersection along a transit corridor immediately adjacent to or within a commercial or mixed use development, or the intersection of two transit corridors immediately adjacent to or within a commercial or mixed use development defines transit-oriented development, or a TOD.

Cottage: an edgeward building type. A single-family dwelling, on a regular lot, often shared with an ancillary building in the reyard.

Courtyard Building: a building that occupies the boundaries of its lot while internally defining one or more private patios.

Curb: the edge of the vehicular pavement detailed as a raised curb or flush to a swale. The Curb usually incorporates the drainage system (see Table 4).

DDC: Design and Development Center. See **UDC**.

Density: the number of dwelling units within a standard measure of land area, usually given as units per acre (see Section 3.4) for residential occupancies, and 1 employee per 200 square feet of building for office and retail use, or 1 employee per 500 square feet of building for manufacturing or other industrial uses, or as determined by the City Of Albuquerque.

Design Speed: is the velocity at which a thoroughfare tends to be driven without the constraints of signage or enforcement. There are three ranges of speed: Very Low: (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High: (above 35 MPH). Lane width is determined by desired design speed.

Developable areas: residual to the Preserved Open Sector.

District: see **Special District**.

Driveway: a vehicular lane within a lot, usually leading to a garage. A Driveway in the First Layer may be used for parking if it is no more than 18 feet wide, thereby becoming subject to the constraints of a parking lot.

Edgeyard Building: a building that occupies the center of its lot with setbacks on all sides.

Elevation: an exterior wall of a building not along a Frontage Line. See: **Facade** (Table 16)

Enfront: to place an element along a frontage line, as in "porches enfront the street."

Entrance, Principal: the main point of access of pedestrians into a building.

Estate House (Syn.: Country house, Villa): an edgeward building type. A single-family dwelling on a very large lot of rural character, often shared by one or more ancillary buildings

Facade: the exterior wall of a building that is set along a Frontage Line (see **Elevation; Frontage Line**).

Frontage Line: those lot lines that coincide with a public frontage. Facades along Frontage Lines define the public realm and are therefore more regulated than the elevations that coincide with other Lot Lines (see Table 16).

GIS (Geographic Information System): a computerized program in widespread municipal use that organizes data on maps. Various municipal departments can input information including the location of wetlands, thoroughfares, water/sewer lines, boundaries, building footprints, schools, zoning, land-use, etc. GIS makes information available as layered databases. The protocol for preparing a Sector Plan should be based on GIS information (Section 2.1).

Greenfield: a project planned for an undeveloped area outside the existing urban fabric. See **Infill**.

Greenway: an open space corridor in largely natural conditions which may include Trails for bicycles and pedestrians.

Greyfield: an area previously used primarily as a parking lot. Shopping centers and shopping malls are typical Greyfield sites.

Growth Sector: one of the three Sectors for New Communities or the Infill Sector, where development is permitted by right.

Hamlet: See CLD.

Home Occupation: non-retail commercial enterprises permitted in Zones T3-6. The work quarters should be invisible from the frontage, located either within the house or in an outbuilding. Permitted activities are defined by the Restricted Office category (Table 11).

House (Syn.: Single): an edgeward building type. A single-family dwelling on a large lot, often shared with an ancillary building in the rearyard.

Independent Building: a building designed by a different architect from the adjacent buildings.

Infill: a project within existing urban fabric.

Inside Turning Radius: the curved edge of a thoroughfare at an intersection, measured at the inside edge of the vehicular tracking. The smaller the Turning Radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn. (See Tables 3 and 16)

Long Pedestrian Shed: A Pedestrian Shed of 1/2 mile radius used for mapping community types when a transit stop (bus or rail) is present or proposed as the Common Destination. People have been shown to walk ten minutes to transit. **See Pedestrian Shed.**

Layer: a range of depth of a lot within which certain elements are permitted (see Table 16).

Linear Pedestrian Shed: A Pedestrian Shed that is elongated along an important Commercial corridor such as a main street. The resulting shed is shaped like a lozenge. (Sometimes called an Elongated Pedestrian Shed.)

Liner Building: a building specifically designed to mask a parking lot or a parking garage from a frontage. A Liner Building, if less than 30 feet deep and two stories, shall be exempt from parking requirements.

Live-Work: a fee-simple dwelling unit that contains a Commercial component anywhere in the unit. (Syn.: Flexhouse.) (See **Work-Live.**)

Lodging: premises available for daily and weekly renting of bedrooms. The area allocated for food service shall be calculated and provided with parking according to retail use.

Lot Line: the boundary that legally and geometrically demarcates a lot (see Frontage Line). Such lines appear graphically on Community and Site Plans. Codes reference lot lines as the baseline for measuring setbacks (see Tables 16 and 14G).

Lot Width: the length of the principal Frontage Line of a lot.

Manufacturing: premises available for the creation, assemblage and/or repair of artifacts, using table-mounted electrical machinery and including their retail sale.

Meeting Hall: a building available for gatherings, including conferences. It should accommodate at least one room equivalent to a minimum of 10 square feet per projected dwelling unit within the pedestrian shed in which the meeting hall is located. A Meeting Hall shall be completed upon the sale of 75% of the dwelling units. The Meeting Hall may be used for the marketing purposes of the development until the sale of 75% of the dwelling units, at which time control of its use shall be given to the [Community Council].

Mixed Use: multiple functions within the same building through superimposition or adjacency, or in multiple buildings within the same area by adjacency. Mixed use is one of the principles of TND development from which many of its benefits are derived, including compactness, pedestrian activity, and parking space reduction.

Neighborhood: an urbanized area at least 40 acres that is primarily Residential. A Neighborhood shall be based upon a partial or entire Standard Pedestrian Shed. The physical center of the Neighborhood should be located at an important traffic intersection associated with a Civic or Commercial institution.

Net Developable Area, Net Site Area: the developable areas of a site. The Net Site Area shall be allocated to the various Transect Zones according to the parameters in Table 14A.

Office: premises available for the transaction of general business but excluding retail, artisanal and manufacturing uses.

Open Sector: One of the two Sectors where development is not permitted.

Outbuilding: an accessory building, usually located towards the rear of the same lot as a Principal Building. It is sometimes connected to the principal building by a Backbuilding. Outbuildings shall not exceed 600 square feet of habitable space, excluding parking areas (see Table 16).

Parking Structure: a building containing two or more stories of parking. Parking Structures shall have Liner Buildings at the first story or higher.

Passage (PS): a pedestrian connector passing between buildings, providing shortcuts through long blocks and connecting rear parking areas to frontages. Passages may be roofed over.

Path (PT): a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

Pedestrian Shed: An area, approximately circular, that is centered on a Common Destination. A Pedestrian Shed is applied to determine the approximate size of a Neighborhood. A Standard Pedestrian Shed is 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most people will walk this distance rather than drive. The outline of the shed must be refined according to actual site conditions, particularly along Thoroughfares. The Common Destination should have the present or future capacity to accommodate a T5 Transect Zone for TND and a T6 Zone for TOD. A Long Pedestrian Shed is 1/2 mile radius or 2640 feet, and may be used for mapping when transit is present or proposed. (Sometimes called a “walkshed” or “walkable catchment.”) A Linear Pedestrian Shed is elongated to follow a Commercial corridor. See **Standard, Long, or Linear Pedestrian Shed**.

Planter: the element of the public streetscape which accommodates street trees and native plantings. Planters may be continuous or individual.

Primary-Secondary Grid: thoroughfare designations appearing on the Regulating Plan. Buildings on the P-Grid are subject to all of the provisions of this Code. Buildings on the S-Grid are exempt from certain provisions, allowing for City Of Albuquerque open parking lots, unlined parking decks, drive-throughs and hermetic building fronts.

Principal Building: the main building on a lot, usually located toward the frontage (see Table 16).

Private Frontage: the privately held layer between the frontage line and the principal building facade. The structures and landscaping within the Private Frontage may be held to specific standards. The variables of Private Frontage are the depth of the setback and the combination of architectural elements such as fences, stoops, porches and galleries (see Table 7).

Public Frontage: the area between the curb of the vehicular lanes and the Frontage Line. Elements of the Public Frontage include the type of curb, walk, planter, street tree and streetlight (see Table 4).

Rear Alley (AL): a vehicular driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys should be paved from building face to building face, with drainage by inverted crown at the center or with roll curbs at the edges.

Rear Lane (LA): a vehicular driveway located to the rear of lots providing access to parking and outbuildings and containing utility easements. Rear lanes may be paved lightly to driveway standards. Its streetscape consists of gravel or landscaped edges, no raised curb and is drained by percolation.

Rearyard Building: a building that occupies the full frontage line, leaving the rear of the lot as the sole yard. This is a more urban type, as the continuous facade spatially defines the public thoroughfare. For its residential function, this type yields a rowhouse. For its commercial function, the rear yard can accommodate substantial parking.

Center Development: a Community Type based upon a partial or entire Long Pedestrian Shed, oriented toward a strong Town Center. With transit existing or proposed, it is called TOD (Transit-Oriented Development). The minimum developable area of a new TOD is 160 acres.

Residential: premises available for long-term human dwelling.

Retail: premises available for the sale of merchandise and food service.

Retail Frontage Line: Frontage Lines designated on a Community Plan that require the provision of a Shopfront, causing the ground level to be available for retail use.

Road (RD): a local, rural and suburban thoroughfare of low vehicular speed and capacity. Its public frontage consists of swales drained by percolation and a walking path or bicycle trail along one or both sides. The landscaping consists of multiple species composed in naturalistic clusters. This type is allocated to the more rural Transect Zones (T1-T3).

Rowhouse: a single-family dwelling that shares a party wall with another of the same type and occupies the full frontage line (Syn: Townhouse; see **Rearyard Building**).

Setback: the area of a lot measured from the lot line to a building facade or elevation. This area must be maintained clear of permanent structures with the exception of: galleries, fences, garden walls, arcades, porches, stoops, balconies, bay windows, terraces and decks (that align with the first story level) which are permitted to encroach into the Setback (see Section 3.2.1 and Table 12G).

Shared Parking Policy: an accounting for parking spaces that are available to more than one function. The requirement is reduced by a factor, shown as a calculation. The Shared Parking ratio varies according to multiple functions in close proximity which are unlikely to require the spaces at the same time (see Tables 11 and 12).

Sideyard Building: a building that occupies one side of the lot with a setback to the other side.

Sidewalk: the paved layer of the public frontage dedicated exclusively to pedestrian activity.

Specialized Building: a building that is not subject to Residential, Commercial, or Lodging classification. Most specialized buildings are dedicated to manufacturing and transportation, and are distorted by the trajectories of machinery.

Special District (SD): Special District designations shall be assigned to areas that, by their intrinsic function, disposition, or configuration, cannot conform to one of the six normative Transect Zones or four Community Types specified by this Code. Typical Districts may include large parks, institutional campuses, refinery sites, airports, etc.

Special Employment District (ED): District designated for employment uses including light, medium, and heavy manufacturing, storage, distribution, and transportation that requires physical separation from other uses.

Standard Pedestrian Shed: An area, approximately circular, that is centered on a Common Destination. A Pedestrian Shed is applied to determine the approximate size of a Neighborhood. A Standard Pedestrian Shed is 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most people will walk this distance rather than drive. The outline of the shed must be refined according to actual site conditions, particularly along thoroughfares. (Sometimes called a “walkshed” or “walkable catchment.”) See **Pedestrian Shed**.

Story: a habitable level within a building of no more than 14 feet in height from finished floor to finished ceiling. Attics and raised basements are not considered stories for the purposes of determining building height.

Streamside Corridor: the zone within which a waterway flows, its width to be variably interpreted according to the Transect Zone.

Street (ST): a local urban thoroughfare of low speed and capacity. Its public frontage consists of raised curbs drained by inlets and sidewalks separated from the vehicular lanes by a planter and parking on both sides. The landscaping consists of regularly placed native plantings. This type is permitted within the more urban Transect Zones (T4-T6).

Streetscape: the urban element that establishes the major part of the public realm. The streetscape is composed of thoroughfares (travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).

Streetscreen: sometimes called Streetwall. A freestanding wall built along the frontage line, or coplanar with the facade, often for the purpose of masking a parking lot from the thoroughfare. Streetscreens [should] be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building facade. The streetscreen may be a hedge or fence by City Of Albuquerque. Streetscreens shall have openings no larger than is necessary to allow automobile and pedestrian access. In addition, all streetscreens over [4 feet] high should be [30%] permeable or articulated to avoid blank walls.

Substantial Modification: alterations to a building that are valued at more than 50% of the replacement cost of the entire building, if new.

Terminated Vista: a location at the axial conclusion of a thoroughfare. A building located at a Terminated Vista designated on a Community Plan is required to be designed in response to the axis.

Third Place: a private building that includes a space conducive to unstructured social gathering. Third Places are usually bars, cafés, and corner stores.

Thoroughfare: a vehicular way incorporating moving lanes and parking lanes within a right-of-way (see Tables 3 and 16).

Tier: synonym for Sector.

TND or Traditional Neighborhood Development: a Community Type based upon a Standard Pedestrian Shed oriented toward a Common Destination consisting of a mixed-use center or corridor, and having a minimum developable area of 80 acres. This Community Type is permitted by right within the G-2 Controlled Growth Sector, the G-3 Intended Growth Sector (see Section 3.3.2) and the G-4 Infill Growth Sector (see Section 4.3.2). A TND may be comprised of a partial or entire Standard Pedestrian Shed or more than one Standard Pedestrian Shed. (Syn.: Village, Urban Village).

TOD: Transit-Oriented Development. A commercial or mixed use center along a transit corridor, a major intersection along a transit corridor immediately adjacent to or within a commercial or mixed use development, or the intersection of two transit corridors immediately adjacent to or within a commercial or mixed use development defines transit-oriented development, or a regional center development served by transit.

Town: TOD. A Community Type consisting of at least one Long Pedestrian Shed with a strong mixed-use center, or more than one TND sharing a center.

Town Center: the mixed-use center or main Commercial corridor of a community. A Town Center in a hamlet or small TND may consist of little more than a meeting hall, corner store, and main civic space. A Town Center for TOD or TOD communities may be a substantial downtown Commercial area, often connected to other Town Centers by transit.

Townhouse: Syn. **Rowhouse.** (See **Rearyard Building.**)

Transect: a system of ordering human habitats in a range from the most natural to the most urban. The SmartCode is based upon six Transect Zones which describe the physical character of place at any scale, according to the density and intensity of land use and urbanism.

Transect Zone (T-Zone): Transect Zones are administratively similar to the land-use zones in conventional codes, except that in addition to the usual building use, density, height, and setback requirements, other elements of the intended habitat are integrated, including those of the private lot and building and the enfronting public streetscape. The elements are determined by their location on the Transect scale. The T-Zones are: T1 Natural, T2 Rural, T3 Sub-Urban, T4 General Urban, T5 Urban Center, and T6 Urban Core (see Table 1).

Transition Line: a horizontal line spanning the full width of a facade, expressed by a material change or by a continuous horizontal articulation such as a cornice or a balcony.

Transportation Corridor: a lineal geographic system incorporating transportation trajectories. A transportation corridor may be a lineal urban Transect Zone. A commercial or mixed use center along a transit corridor, a major intersection along a transit corridor immediately adjacent to or within a commercial or mixed use development, or the intersection of two transit corridors immediately adjacent to or within a commercial or mixed use development defines transit-oriented development, or a TOD.

Type: a category determined by function, disposition, and configuration, including size or extent. There are community types, street types, civic space types, etc. (See also: **Building Type**.)

Urban Village: A TND Community Type within an urbanized area. See: **TND**.

Village: A Village is usually a TND Community Type standing isolated in the countryside, but with a stronger center than a hamlet due to its proximity to a transportation corridor. See: **TND**.

Work-Live: a fee-simple mixed-use unit with a substantial Commercial component that may accommodate employees and walk-in trade. Therefore the unit shall require ADA compliance for accessibility. (Syn.: Live-With.) (See **Live-Work**.)